

**CITY OF RENTON**DEPARTMENT OF COMMUNITY AND
ECONOMIC DEVELOPMENT
REPORT TO THE HEARING EXAMINER, EXHIBITS

Project Name: Quendall Terminals		Project Number: LUA09-151 ECF, EIS, SA-M, SM	
Date of Hearing April 19, 2016	Staff Contact Vanessa Dolbee Current Planning Manager	Project Contact/Applicant Campbell Mathewson, Century Pacific, L. P., 1201 Third Ave, suite 1680, Seattle, WA 98101	Project Location SW ¼ Section 29, Township 24 N, Range 5 E. Parcel 2924059002. South of the Seahawks Training Facility

The following exhibits were entered into the record:

- Exhibit 1: Staff report to the Hearing Examiner, which can be found at the following link:
<http://rentonwa.gov/business/default.aspx?id=32800>
- Exhibit 2: Environmental Review Documents – Draft EIS, Addendum to the Draft EIS, FEIS and Mitigation Document, which can be found at the following link:
<http://rentonwa.gov/business/default.aspx?id=32800>
- Exhibit 3: Environmental Review Committee Signature Sheets
- Exhibit 4: Neighborhood Detail Map
- Exhibit 5: Binding Site Plan
- Exhibit 6: Joint Stipulation and Proposed Order Dismissing Appeal and e-mail chains following request.
- Exhibit 7: Site Plan (black and white and color)
- Exhibit 8: Parking Plan (black and white and color)
- Exhibit 9: Area Outline of Spaces
- Exhibit 10: Elevations
- Exhibit 11: Conceptual Landscape Plan
- Exhibit 12: Conceptual Storm Drainage and Grading Plan
- Exhibit 13: Roadway Sections
- Exhibit 14: Conceptual Utility Plan
- Exhibit 15: EA Letter addressing EPA and public involvement in the process
- Exhibit 16: Advisory Notes/Plan Review Comments
- Exhibit 17: Concurrence Memo
- Exhibit 18: Additional Lanes Required

NOTICE OF ISSUANCE AND AVAILABILITY
FINAL ENVIRONMENTAL IMPACT STATEMENT (FEIS) AND MITIGATION
DOCUMENT

Notice is hereby given under WAC 197-11-510 and RMC 4-9-070 that the Final Environmental Impact Statement (FEIS) and Mitigation Document for the proposal described below was issued by the City of Renton Environmental Review Committee on Monday, August 31, 2015, and is available for public review. Copies are available for review at the Renton Main Library, located at 100 Mill Avenue South, and the Renton Highlands Branch Library, located at 2902 NE 12th Street, and at Renton City Hall, Customer Service Counter, 6th floor, 1055 South Grady Way, Renton WA 98057, and on the City of Renton web site: (www.rentonwa.gov).

APPLICATION NUMBER(S): LUA09-151, EIS, ECF, BSP, SA-M, SM

PROJECT PROPONENT: Campbell Mathewson
Century Pacific, L. P.
1201 Third Ave, Suite 1680
Seattle, WA 98101

PROJECT NAME: Quendall Terminals

DESCRIPTION OF PROJECT: The proposal is located adjacent to Lake Washington on 21.46 acres of Commercial/Office/Residential (COR) zoned property. The EIS evaluates potential impacts resulting from a mixed-use development project, including four Alternatives, of which considers no action. The Preferred Alternative would contain 21,600 square feet of retail space, 9,000 square feet of restaurant and 692 residential units.

LOCATION OF PROPOSAL: 4350 Lake Washington Blvd

LEAD AGENCY: City of Renton
Environmental Review Committee
Department of Community & Economic Development
Planning Division

RESPONSIBLE OFFICIAL: Environmental Review Committee
Department of Community & Economic Development
Planning Division
1055 S Grady Way
Renton, WA 98057

DOCUMENT PURCHASE INFORMATION: The Final Environmental Impact Statement is available for purchase from the Finance Department on the 1st Floor of Renton City Hall for \$35 per hard copy or \$10.00 per CD, plus tax and postage (if mailed).

PUBLIC REVIEW: The impacts described in the Quendall Terminals DEIS and EIS Addendum are the basis for the mitigation measures established in the Mitigation Document. The Mitigation Document is designated by the City of Renton as the first decision document for the proposal.

APPEAL INFORMATION: Upon issuance of the FEIS and Mitigation Document, a twenty (20) day appeal period commences. Pursuant to WAC 197-11-680 and RMC 4-8-110.E., the adequacy of the Final EIS and the Mitigation Document may be appealed. Appeals must: 1) state specific objections of fact and/or law; 2) be submitted in writing by 5:00 p.m. September 24, 2015; and 3) be accompanied by a filing fee of \$250.00. Appeals must be addressed to Phil Olbrechts, Hearing Examiner, City of Renton, Renton City Hall, 1055 S Grady Way, Renton, WA 98055.

ADDITIONAL INFORMATION: If you would like additional information, please contact Vanessa Dolbee, City of Renton at (425)430-7314 or vdolbee@rentonwa.gov.

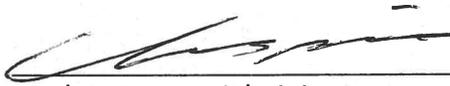
PUBLICATION DATE: September 4, 2015

DATE OF DECISION: August 31, 2015

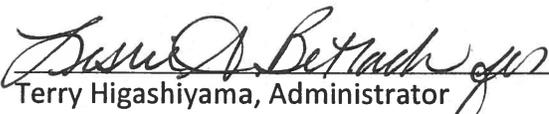
SIGNATURES:


Gregg Zimmerman, Administrator
Public Works Department

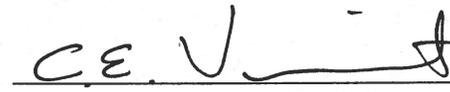
8/31/15
Date


Mark Peterson, Administrator
Fire & Emergency Services

8/31/15
Date


Terry Higashiyama, Administrator
Community Services Department

8-31-15
Date


C.E. "Chip" Vincent, Administrator
Department of Community &
Economic Development

8/31/15
Date

NOTICE OF ISSUANCE AND AVAILABILITY

ENVIRONMENTAL IMPACT STATEMENT ADDENDUM (EIS ADDENDUM)

Notice is hereby given under WAC 197-11-510 and RMC 4-9-070 that the Environmental Impact Statement Addendum (EIS Addendum) for the proposal described below was issued by the City of Renton Environmental Review Committee on Monday, October 15, 2012, and is available for public review and comment. Copies are available for review at the Renton Main Library, located at 100 Mill Avenue South, and the Renton Highlands Branch Library, located at 2902 NE 12th Street, and at Renton City Hall, Customer Service Counter, 6th floor, 1055 South Grady Way, Renton WA 98057, and on the City of Renton web site: (www.rentonwa.gov).

APPLICATION NUMBER(S): LUA09-151, EIS, ECF, BSP, SA-M, SM

PROJECT PROPONENT: Campbell Mathewson
Century Pacific, L. P.
1201 Third Ave, Suite 1680
Seattle, WA 98101

PROJECT NAME: Quendall Terminals

DESCRIPTION OF PROJECT: The proposal is located adjacent to Lake Washington on 21.46 acres of Commercial/Office/Residential (COR) zoned property. The EIS Addendum to the Draft Environmental Impact Statement (DEIS) issued December 10, 2010; evaluates potential impacts resulting from a new Preferred Alternative. Similar to Alternatives 1 and 2 in the DEIS, the Preferred Alternative is intended to be a mixed-use development. The Preferred Alternative would contain 21,600 square feet of retail space, 9,000 square feet of restaurant and 692 residential units. For those assumptions that have been modified under the Preferred Alternative, the updated analysis is included in the provided EIS Addendum. These elements include, Critical Areas, Aesthetics/View, Parks and Recreation, Transportation, Cultural Resources and Relationship to Plans and Policies.

LOCATION OF PROPOSAL: 4350 Lake Washington Blvd

LEAD AGENCY: City of Renton
Environmental Review Committee
Department of Community & Economic Development
Planning Division

RESPONSIBLE OFFICIAL: Environmental Review Committee
Department of Community & Economic Development
Planning Division
1055 S Grady Way
Renton, WA 98057

DOCUMENT PURCHASE INFORMATION: The Draft Environmental Impact Statement is available for purchase from the Finance Department on the 1st Floor of Renton City Hall for \$25 per hard copy or \$10.00 per CD, plus tax and postage (if mailed).

PUBLIC REVIEW: Written public comment on the DEIS will be accepted for a 30-day review period ending at 5:00 p.m. Monday, November 19, 2012. Written Comments should be addressed to: Vanessa Dolbee, Senior Planner, Planning Division, 6th floor Renton City Hall, 1055 South Grady Way, Renton, WA 98057.

ADDITIONAL INFORMATION: If you would like additional information, please contact Vanessa Dolbee, City of Renton at (425)430-7314

PUBLICATION DATE: October 19, 2012

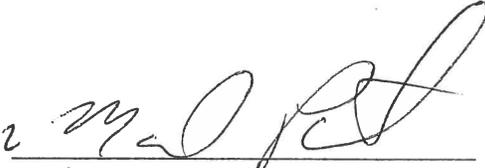
DATE OF DECISION: October 15, 2012

SIGNATURES:



Gregg Zimmerman, Administrator
Public Works Department

10/15/2012
Date



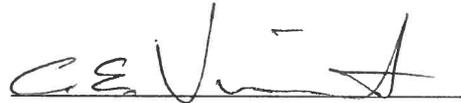
Mark Peterson, Administrator
Fire & Emergency Services

10/15/12
Date



Terry Higashiyama, Administrator
Community Services Department

10/15/12
Date



C.E. "Chip" Vincent, Administrator
Department of Community &
Economic Development

10/15/2012
Date

NOTICE OF ISSUANCE AND AVAILABILITY
DRAFT ENVIRONMENTAL IMPACT STATEMENT (DEIS)

Notice is hereby given that the City of Renton has issued the Draft Environmental Impact Statement (DEIS) for Quendall Terminals mixed use development on December 10, 2010 pursuant to WAC 197-11-510 and RMC 4-9-070, and is available for public review. Copies are available for review at the Renton Main Library, the Renton Highlands Branch Library, and Renton City Hall, Customer Service Counter, 6th floor, 1055 South Grady Way, Renton WA 98055, and on the City of Renton website (www.rentonwa.gov).

APPLICATION NUMBER(S): LUA09-151, EIS, ECF, BSP, SA-M, SM

PROJECT PROPONENT: Campbell Mathewson
Century Pacific, L. P.
1201 Third Ave, Suite 1680
Seattle, WA 98101

PROJECT NAME: Quendall Terminals

DESCRIPTION OF PROJECT: The Quendall Terminals mix use development DEIS considers potential development concepts for the redevelopment of a 21.46 acre Superfund site located along the shoreline of Lake Washington. The DEIS evaluates potential impacts resulting from the proposed development. The following are alternatives evaluated within the DEIS: Alternative 1, which consists of 800 residential units, 245,000 square feet of office, 21,600 square feet of retail and 9,000 square feet of restaurant; Alternative 2, which consist of a less dense alternative where the office component is eliminated and residential units are reduced to 708 units; and Alternative 3, a no action alternative.

LOCATION OF PROPOSAL: 4350 Lake Washington Blvd

LEAD AGENCY: City of Renton
Environmental Review Committee
Department of Community & Economic Development
Planning Division

RESPONSIBLE OFFICIAL: Environmental Review Committee
Department of Community & Economic Development
Planning Division
1055 S Grady Way
Renton, WA 98057

DOCUMENT PURCHASE INFORMATION: The Draft Environmental Impact Statement is available for purchase from the Finance Department on the 1st Floor of Renton City Hall for \$25 per hard copy or \$10.00 per CD, plus tax and postage (if mailed).

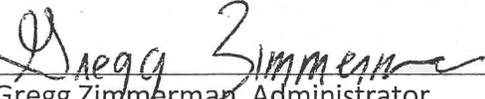
PUBLIC REVIEW: Written public comment on the DEIS will be accepted for a 30-day review period ending at 5:00 p.m. Monday, January 10, 2011. Written Comments should be addressed to: Vanessa Dolbee, Senior Planner, Planning Division, 6th floor Renton City Hall, 1055 South Grady Way, Renton, WA 98057. A public hearing will be held on Tuesday, January 4, 2011, at 6:00 p.m., in the City Council Chambers, 7th floor Renton City Hall, 1055 South Grady Way, Renton, WA.

ADDITIONAL INFORMATION: If you would like additional information, please contact Vanessa Dolbee, City of Renton at (425)430-7314

PUBLICATION DATE: December 10, 2010

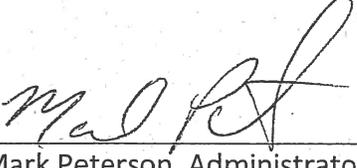
DATE OF DECISION: December 6, 2010

SIGNATURES:



Gregg Zimmerman, Administrator
Public Works Department

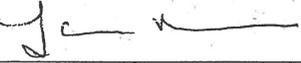
Date



Mark Peterson, Administrator
Fire & Emergency Services

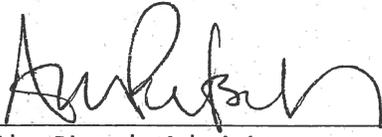
Date

12/6/10



Terry Higashiyama, Administrator
Community Services Department

Date



Alex Pietsch, Administrator
Department of Community &
Economic Development

Date

12/6/10

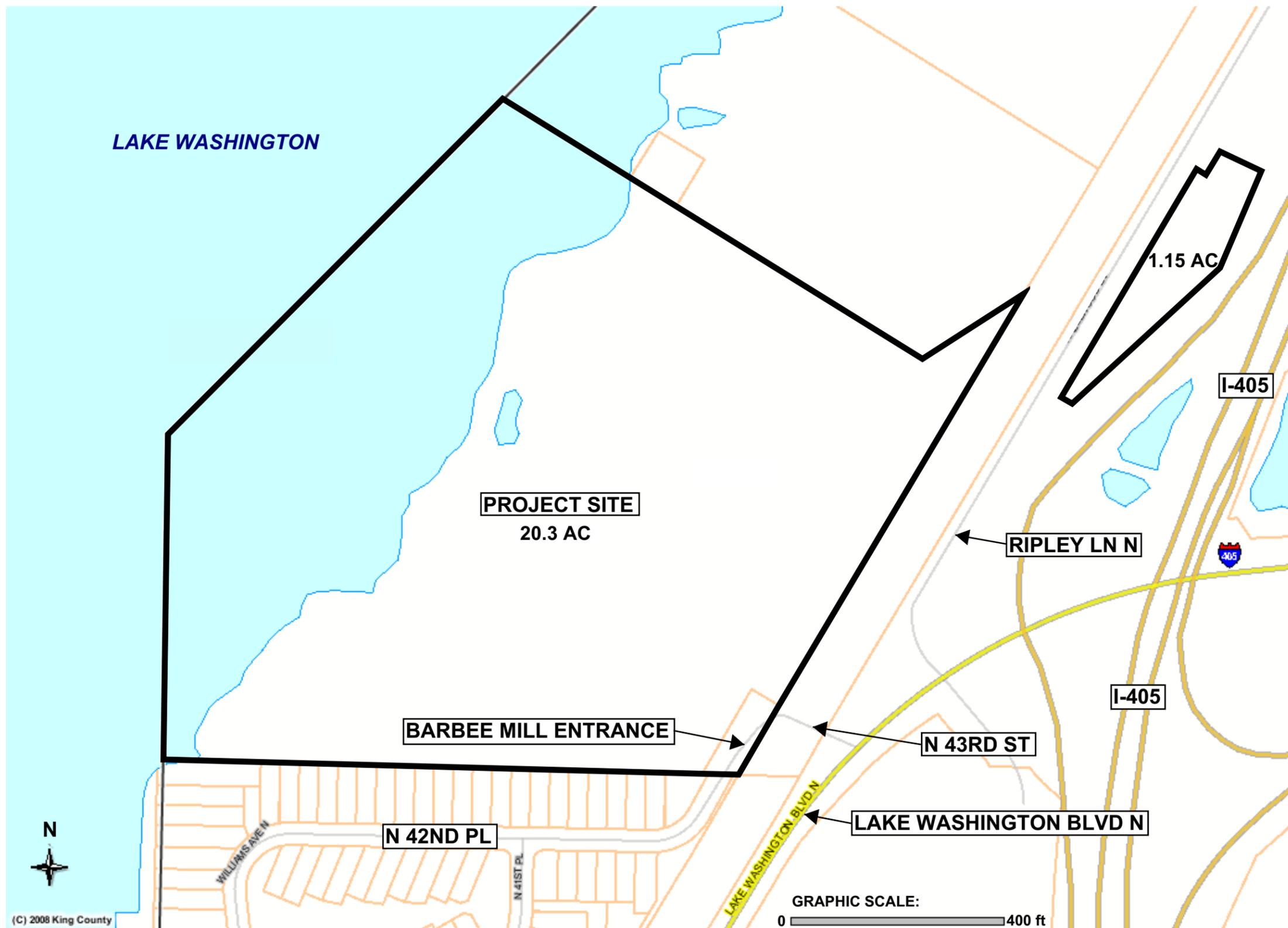


EXHIBIT 4

NEIGHBORHOOD DETAIL MAP
QUENDALL TERMINALS

DESCRIPTION:
THAT PORTION OF GOVERNMENT LOT 5 IN SECTION 29, TOWNSHIP 24 NORTH, RANGE 5 EAST, W.M., AND SHORELAND ADJOINING LYING WESTERLY OF THE NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY AND SOUTHERLY OF A LINE, IN KING COUNTY, WASHINGTON, DESCRIBED AS FOLLOWS:

BEGINNING AT THE QUARTER CORNER ON THE SOUTH LINE OF SAID SECTION 29; THENCE NORTH 89°58'36" WEST ALONG THE SOUTH LINE OF SAID LOT 5, 1,113.01 FEET TO THE WESTERLY LINE OF SAID NORTHERN PACIFIC RAILROAD RIGHT-OF-WAY; THENCE NORTH 29°44'54" EAST 849.62 FEET ALONG SAID RIGHT-OF-WAY LINE TO A POINT HEREINAFTER REFERRED TO AS POINT A; THENCE CONTINUING NORTH 29°44'54" EAST 200.01 FEET TO THE TRUE POINT OF BEGINNING OF THE LINE HEREIN DESCRIBED; THENCE SOUTH 56°28'50" WEST 222.32 FEET TO A POINT WHICH BEARS NORTH 59°24'56" WEST 100.01 FEET FROM SAID POINT A; THENCE NORTH 59°24'56" WEST TO THE INNER HARBOR LINE AND THE END OF SAID LINE DESCRIPTION;

ALSO THAT PORTION OF SAID GOVERNMENT LOT 5 LYING SOUTHEASTERLY OF LAKE WASHINGTON BOULEVARD, WESTERLY OF SECONDARY STATE HIGHWAY NUMBER 2A AND NORTHWESTERLY OF THE RIGHT-OF-WAY OF PUBLIC STATE HIGHWAY NUMBER 1 AS ESTABLISHED BY DEED RECORDED JANUARY 15, 1964 UNDER RECORDING NO. 5687408;

AND EXCEPT THAT PORTION THEREOF CONVEYED TO CITY OF RENTON, A MUNICIPAL CORPORATION BY DEED RECORDED JUNE 19, 2008 UNDER RECORDING NO. 20080619001179.

TITLE REPORT REFERENCE:
THIS SURVEY WAS CONDUCTED ACCORDING TO THE DESCRIPTION SHOWN, FURNISHED BY FIRST AMERICAN TITLE INSURANCE COMPANY, COMMITMENT NO. NCS-380710-WA1, THIRD REPORT, DATED DECEMBER 14, 2009. THE EASEMENTS SHOWN OR NOTED HEREON RELATE TO THIS COMMITMENT.

NOTE: EASEMENTS CREATED OR RESCINDED AFTER THIS DATE ARE NOT SHOWN OR NOTED HEREON.

TITLE REPORT SCHEDULE B EXCEPTIONS:
ITEMS CIRCLED ARE SHOWN ON MAP.

3. RELINQUISHMENT OF ALL EXISTING AND FUTURE RIGHTS TO LIGHT, VIEW AND AIR, TOGETHER WITH THE RIGHTS OF ACCESS TO AND FROM THE STATE HIGHWAY CONSTRUCTED ON LANDS CONVEYED BY DOCUMENT IN FAVOR OF THE STATE OF WASHINGTON:

RECORDED: OCTOBER 16, 1951
RECORDING NO.: 4178247

4. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:

RECORDING INFORMATION: MARCH 20, 1963 UNDER RECORDING NO. 5562896
IN FAVOR OF: PUGET SOUND POWER AND LIGHT COMPANY, A CORPORATION, ITS SUCCESSORS AND ASSIGNS
FOR: POWER LINE
AFFECTS: THE NORTHWESTERLY PORTION OF THE PROPERTY HEREIN DESCRIBED WESTERLY OF RAILROAD RIGHT-OF-WAY

5. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:

RECORDING INFORMATION: NOVEMBER 20, 1964 UNDER RECORDING NO. 5814320
IN FAVOR OF: MUNICIPALITY OF METROPOLITAN SEATTLE, ITS SUCCESSORS AND ASSIGNS
FOR: UTILITY EASEMENT
AFFECTS: THE NORTHEASTERLY PORTION OF THE PROPERTY HEREIN DESCRIBED WESTERLY OF RAILROAD RIGHT-OF-WAY

7. COVENANTS, CONDITIONS, RESTRICTIONS AND/OR EASEMENTS:

RECORDED: JULY 16, 1975
RECORDING NO.: 7507160536

8. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:

RECORDING INFORMATION: JULY 7, 1980 UNDER RECORDING NO. 8007070459
IN FAVOR OF: CITY OF RENTON, A MUNICIPAL CORPORATION
FOR: PUBLIC UTILITIES (INCLUDING WATER AND SEWER)
AFFECTS: PORTION OF THE PROPERTY HEREIN DESCRIBED WESTERLY AND ADJACENT TO RAILROAD RIGHT-OF-WAY

9. SUBJECT TO THE TERMS OF THE JOINT VENTURE AGREEMENT BETWEEN PUGET TIMBER COMPANY AND ALTINO PROPERTIES INC., DATED JUNE 15, 1971, AMENDMENT THERETO DATED SEPTEMBER 12, 1980, BOTH APPEARING UNDER RECORDING NO. 8102190531, RECORDED FEBRUARY 19, 1981, ACCORDING TO RECITAL ON EASEMENT UNDER RECORDING NO. 9602150689, RECORDED FEBRUARY 15, 1996; J.H. BAXTER & CO., CALIFORNIA LIMITED PARTNERSHIP APPEARS TO HAVE SUCCEEDED PUGET TIMBER COMPANY, A JOINT VENTURER. ALL AMENDMENTS TO SAID JOINT VENTURE AGREEMENT MUST BE SUBMITTED PRIOR TO CLOSING TO DETERMINE THE CURRENT SIGNATORIES.

10. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:

RECORDING INFORMATION: FEBRUARY 15, 1996 UNDER RECORDING NO. 9602150689
IN FAVOR OF: BARBEE MILL CO., INC., A WASHINGTON CORPORATION, AND J.H. BAXTER & CO., A CALIFORNIA LIMITED PARTNERSHIP
FOR: ROADWAY AND UTILITIES
AFFECTS: THE EAST 60 FEET ADJACENT TO WEST MARGIN OF RAILROAD RIGHT-OF-WAY

11. THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "LAND CORNER RECORD" RECORDED MAY 29, 1997 AS RECORDING NO. 9705290636 OF OFFICIAL RECORDS.

12. CONDITIONS, NOTES, EASEMENTS, PROVISIONS CONTAINED AND/OR DELINEATED ON THE FACE OF THE SURVEY RECORDED FEBRUARY 9, 2000 UNDER RECORDING NO. 20000209900005, RECORDED IN VOLUME 135 OF SURVEYS, AT PAGE(S) 176, IN KING COUNTY, WASHINGTON.

13. EASEMENT, INCLUDING TERMS AND PROVISIONS CONTAINED THEREIN:

RECORDING INFORMATION: SEPTEMBER 2, 2008 UNDER RECORDING NO. 20080902001178

IN FAVOR OF: CITY OF RENTON, A MUNICIPAL CORPORATION, ITS SUCCESSORS AND ASSIGNS
FOR: SANITARY SEWER UTILITIES AND UTILITY PIPELINES
AFFECTS: AS DESCRIBED THEREIN

14. PRIVATE ACCESS TO SAID PREMISES IS ACROSS A RAILROAD RIGHT-OF-WAY. THIS COMPANY WILL REQUIRE THAT THE "PRIVATE ROADWAY AND CROSSING AGREEMENT", AND ANY ASSIGNMENTS OR MODIFICATIONS THEREOF WHICH WERE ISSUED BY THE RAILROAD COMPANY, BE SUBMITTED FOR EXAMINATION. THE COVERAGE THEN AFFORDED UNDER ANY POLICY(IES) ISSUED, RELATIVE TO ACCESS TO SAID PREMISES, WILL BE LIMITED BY THE RESTRICTIONS, CONDITIONS AND PROVISIONS AS CONTAINED THEREIN. IF NO "AGREEMENT" EXISTS, THE FORTHCOMING POLICY(IES) WILL CONTAIN THE FOLLOWING EXCEPTION:

THE LACK OF RIGHT OF ACCESS TO AND FROM THE LAND ACROSS A RAILROAD RIGHT-OF-WAY.

15. ANY QUESTION AS TO THE TRUE LOCATION OF THE LATERAL BOUNDARIES OF THE SAID 2ND CLASS SHORELANDS.

16. RIGHT OF THE STATE OF WASHINGTON IN AND TO THAT PORTION, IF ANY, OF THE PROPERTY HEREIN DESCRIBED WHICH LIES BELOW THE LINE OF ORDINARY HIGH WATER OF LAKE WASHINGTON.

17. RIGHTS OF THE GENERAL PUBLIC TO THE UNRESTRICTED USE OF ALL THE WATERS OF A NAVIGABLE BODY OF WATER NOT ONLY FOR THE PRIMARY PURPOSE OF NAVIGATION, BUT ALSO FOR COROLLARY PURPOSES; INCLUDING (BUT NOT LIMITED TO) FISHING, BOATING, BATHING, SWIMMING, WATER SKIING AND OTHER RELATED RECREATIONAL PURPOSES, AS THOSE WATERS MAY AFFECT THE TIDELANDS, SHORELANDS OR ADJOINING UPLANDS AND WHETHER THE LEVEL OF THE WATER HAS BEEN RAISED NATURALLY OR ARTIFICIALLY TO A MAINTAINED OR FLUCTUATING LEVEL, ALL AS FURTHER DEFINED BY THE DECISIONAL LAW OF THIS STATE. (AFFECTS ALL OF THE PREMISES SUBJECT TO SUCH SUBMERGENCE)

18. TERMS, CONDITIONS, PROVISIONS AND STIPULATIONS OF THE JOINT VENTURE AGREEMENT OF QUENDALL TERMINALS, A WASHINGTON JOINT VENTURE. A COPY OF THE CURRENT AGREEMENT AND ANY AMENDMENTS MUST BE SUBMITTED PRIOR TO CLOSING. ANY CONVEYANCE OR ENCUMBRANCE OF THE JOINT VENTURE PROPERTY MUST BE EXECUTED BY ALL OF THE JOINT VENTURERS.

19. TITLE TO VEST IN AN INCOMING OWNER WHOSE NAME IS NOT DISCLOSED. SUCH NAME MUST BE FURNISHED TO US SO THAT A NAME SEARCH MAY BE MADE.

20. THE TERMS AND PROVISIONS CONTAINED IN THE DOCUMENT ENTITLED "ORDINANCE NO. 5463" RECORDED AUGUST 10, 2009 AS 20090810001984 OF OFFICIAL RECORDS.

21. UNRECORDED LEASEHOLDS, IF ANY, RIGHTS OF VENDORS AND SECURITY AGREEMENT ON PERSONAL PROPERTY AND RIGHTS OF TENANTS, AND SECURED PARTIES TO REMOVE TRADE FIXTURES AT THE EXPIRATION OF THE TERM.

ADDRESSES:

OWNER:
CENTURY PACIFIC, INC. AND J.H. BAXTER & CO.
ATTN: ROBERT CUGINI
800 S. THIRD STREET
RENTON, WA 98057

APPLICANT:
CENTURY PACIFIC, L.P.
ATTN: CAMPBELL MATHESON
1201 3RD AVENUE, SUITE 1680
SEATTLE, WA 98101

LICENSED LAND SURVEYOR:
BUSH, ROED AND HITCHINGS, INC.
ATTN: DAKIN A. BELL
2009 MINOR AVENUE EAST
SEATTLE, WA 98102

ENGINEER:
KPFF CONSULTING ENGINEERS
ATTN: TOM JONES
1601 FIFTH AVENUE, SUITE 1600
SEATTLE, WA 98101

SITE ADDRESS:
4350 LAKE WASHINGTON BOULEVARD
RENTON, WASHINGTON

TAX ACCOUNT NO.:
292405-9002-03

HORIZONTAL DATUM:
NAD 83/91; CITY OF RENTON
MONUMENTS SHOWN HEREON WERE
VISITED IN OCTOBER 1999 USING A WILD
TC1000U ELECTRONIC THEODELITE, SERIAL
NUMER 337190.

ZONING AND SETBACKS:

ZONING : COMMERCIAL/OFFICE/RETAIL

SETBACKS: DETERMINED THROUGH SITE
DEVELOPMENT PLAN REVIEW. PER CITY
OF RENTON MUNICIPAL CODE 4-2-120B.

DECLARATION:

KNOW ALL PEOPLE BY THESE PRESENTS THAT WE, THE UNDERSIGNED OWNERS OF INTEREST IN THE LAND HEREIN DESCRIBED MAKE A BINDING SITE PLAN PURSUANT TO CHAPTER 58.17 R.C.W. AND DECLARE THE BINDING SITE PLAN TO BE A GRAPHIC REPRESENTATION OF THE SAME AND THAT SAID BINDING SITE PLAN IS MADE WITH FREE CONSENT AND IN ACCORDANCE WITH THE DESIRE OF THE OWNERS.

IN WITNESS WHEREOF WE SET HANDS AND SEALS;

CENTURY PACIFIC, L.P.

BY _____

DATE _____

APPROVALS:

CITY OF RENTON ADMINISTRATOR OF PLANNING / BUILDING / PUBLIC WORKS

EXAMINED AND APPROVED PER RCW 58.17.160 (1) THIS _____ DAY OF _____ 20____

ADMINISTRATOR, CITY OF RENTON

KING COUNTY DEPARTMENT OF ASSESSMENTS

EXAMINED AND APPROVED THIS _____ DAY OF _____ 20____

KING COUNTY ASSESSOR

DEPUTY KING COUNTY ASSESSOR

KING COUNTY FINANCE DIVISION CERTIFICATION

I HEARBY CERTIFY THAT ALL PROPERTY TAXES ARE PAID, THAT THERE ARE NO DELINQUENT SPECIAL ASSESSMENTS CERTIFIED TO THIS OFFICE FOR COLLECTION AND THAT ALL SPECIAL ASSESSMENTS CERTIFIED TO THIS OFFICE FOR COLLECTION ON ANY OF THE PROPERTY HEREIN CONTAINED DEDICATED AS STREETS, ALLEYS OR ANY OTHER PUBLIC USE, ARE PAID IN FULL.

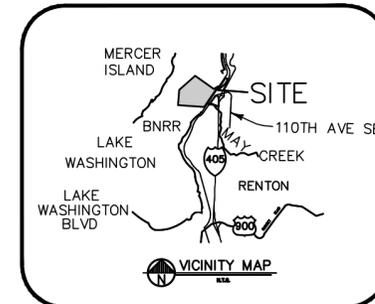
THIS _____ DAY OF _____ 20____

DIRECTOR, KING COUNTY OFFICE OF FINANCE

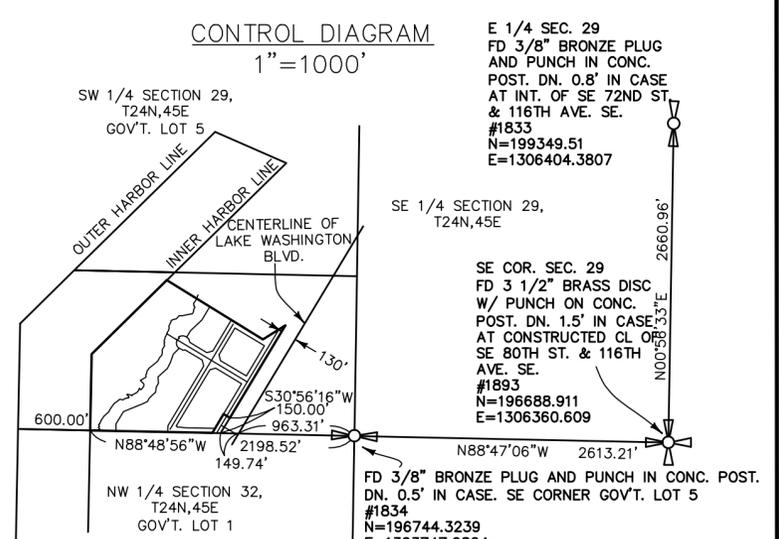
DEPUTY DIRECTOR, KING COUNTY OFFICE OF FINANCE

AREA TABLE	
PARCEL	AREA
AREA OF TOTAL PARCEL PER LEGAL DESCRIPTION	1,375,165 SQ. FT. OR 31.5694 AC.
LOT 1	50,823 SQ. FT. OR 1.1667 AC.
LOT 2	75,548 SQ. FT. OR 1.7343 AC.
LOT 3	83,819 SQ. FT. OR 1.9242 AC.
LOT 4	146,774 SQ. FT. OR 3.3818 AC.
LOT 5	218,794 SQ. FT. OR 5.0228 AC.
LOT 6	109,485 SQ. FT. OR 2.5134 AC.
LOT 7	50,725 SQ. FT. OR 1.1645 AC.
PRIVATE ST. D	15,529 SQ. FT. OR 0.3442 AC.
PRIVATE ST. E	12,636 SQ. FT. OR 0.2901 AC.
PUBLIC STREETS A, B AND C	161,243 SQ. FT. OR 3.7016 AC.
PORTION OF TOTAL PARCEL LYING WITHIN LAKE WASHINGTON	449,789 SQ. FT. OR 10.3257 AC.

NOTES:
-LOTS 1 AND 6 ARE BOUNDED ON THE WEST BY THE SHORE OF LAKE WASHINGTON, SHOWN AS THE "OHWM" (ORDINARY HIGH WATER MARK).
-43,833 SQ FT OF LOT 1 LIES WITHIN THE 100' SHORELINE SETBACK.
-106,703 SQ FT OF LOT 6 LIES WITHIN THE 100' SHORELINE SETBACK.



REFERENCES:
1. RECORD OF SURVEY RECORDED UNDER RECORDING NUMBER 20000209900005, RECORDS OF KING COUNTY, STATE OF WASHINGTON.



E 1/4 SEC. 29
FD 3/8" BRONZE PLUG AND PUNCH IN CONC.
POST. DN. 0.8' IN CASE AT INT. OF SE 72ND ST. & 116TH AVE. SE.
#1833
N=199349.51
E=1306404.3807

SE COR. SEC. 29
FD 3 1/2" BRASS DISC W/ PUNCH ON CONC.
POST. DN. 1.5' IN CASE AT CONSTRUCTED CL OF SE 80TH ST. & 116TH AVE. SE.
#1893
N=196688.911
E=1306360.609

FD 3/8" BRONZE PLUG AND PUNCH IN CONC. POST. DN. 0.5' IN CASE. SE CORNER GOV'T. LOT 5
#1834
N=196744.3239
E=1303747.9864

AUDITOR OR RECORDER'S CERTIFICATE
Filed for record this _____ day of _____, 20____ at _____ M.
in Book _____ of Surveys at page _____ at the request of
BUSH, ROED & HITCHINGS, INC.
COUNTY AUDITOR OR DIVISION OF RECORDS & ELECTIONS

County Auditor or _____ Superintendent of Records

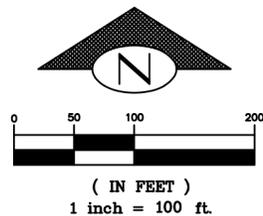
SURVEYOR'S CERTIFICATE
This map correctly represents a survey made by me or under my direction in conformance with the requirements of the Survey Recording Act at the request of
CENTURY PACIFIC, L.P. _____ in _____, 20____

Certificate No. 37546

BUSH, ROED & HITCHINGS, INC.
CIVIL ENGINEERS & LAND SURVEYORS
2009 MINOR AVENUE EAST
SEATTLE, WASHINGTON
98102-3513
(206) 323-4144
1-800-935-0508
FAX# (206) 323-7135

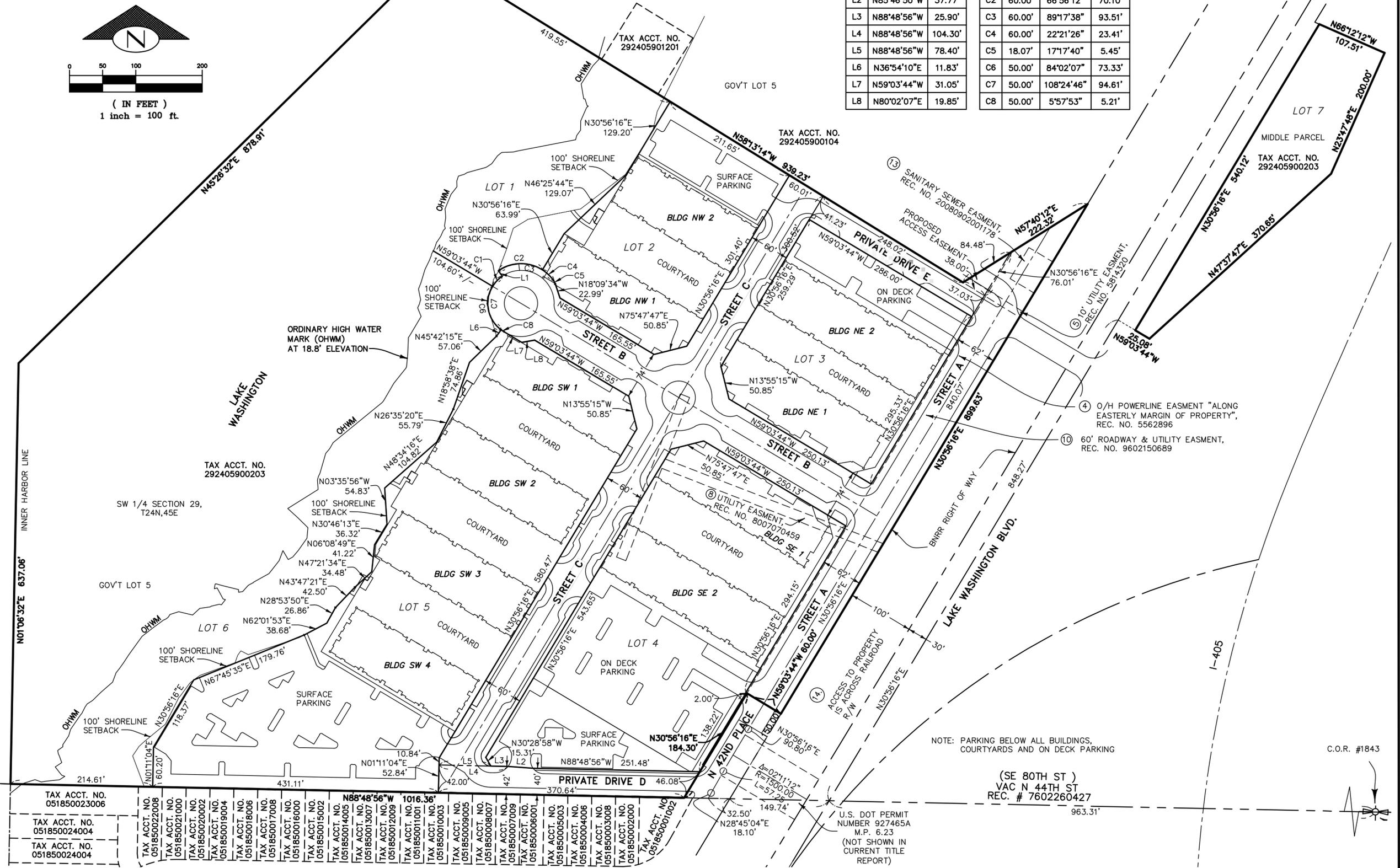
BINDING SITE PLAN - QUENDALL TERMINALS
CENTURY PACIFIC, L.P.
CITY OF RENTON, KING COUNTY, STATE OF WASHINGTON

DRAWN BY LMK/TRS	DATE: 01/11/16	JOB # 2009050.04
CHECKED BY DAB	SCALE: NA	SHEET 1 OF 5



LINE TABLE		
NO.	BEARING	LENGTH
L1	N34°41'04"W	10.00'
L2	N85°46'50"W	37.77'
L3	N88°48'56"W	25.90'
L4	N88°48'56"W	104.30'
L5	N88°48'56"W	78.40'
L6	N36°54'10"E	11.83'
L7	N59°03'44"W	31.05'
L8	N80°02'07"E	19.85'

CURVE TABLE			
NO.	RADIUS	DELTA	LENGTH
C1	50.00'	24°22'39"	21.27'
C2	60.00'	66°56'12"	70.10'
C3	60.00'	89°17'38"	93.51'
C4	60.00'	22°21'26"	23.41'
C5	18.07'	17°17'40"	5.45'
C6	50.00'	84°02'07"	73.33'
C7	50.00'	108°24'46"	94.61'
C8	50.00'	5°57'53"	5.21'



TAX ACCT. NO. 051850023006	TAX ACCT. NO. 051850022008	TAX ACCT. NO. 051850021000	TAX ACCT. NO. 051850020002	TAX ACCT. NO. 051850019004	TAX ACCT. NO. 051850018006	TAX ACCT. NO. 051850017008	TAX ACCT. NO. 051850016000	TAX ACCT. NO. 051850015002	TAX ACCT. NO. 051850014005	TAX ACCT. NO. 051850013007	TAX ACCT. NO. 051850012009	TAX ACCT. NO. 051850011001	TAX ACCT. NO. 051850010003	TAX ACCT. NO. 051850009005	TAX ACCT. NO. 051850008007	TAX ACCT. NO. 051850007009	TAX ACCT. NO. 051850006001	TAX ACCT. NO. 051850005003	TAX ACCT. NO. 051850004006	TAX ACCT. NO. 051850003008	TAX ACCT. NO. 051850002000	TAX ACCT. NO. 051850001002
----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------	----------------------------

(SE 80TH ST)
VAC N 44TH ST
REC. # 7602260427

U.S. DOT PERMIT
NUMBER 927465A
M.P. 6.23
(NOT SHOWN IN
CURRENT TITLE
REPORT)

NW 1/4 SECTION 32, T24N, 45E
GOV'T LOT 1
PLAT OF BARBEE MILL
VOL. 246, PGS. 25-39,
REC. NO.
20080208000182



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2009 MINOR AVENUE EAST
SEATTLE, WASHINGTON
98102-3513
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FAX# (206) 323-7135

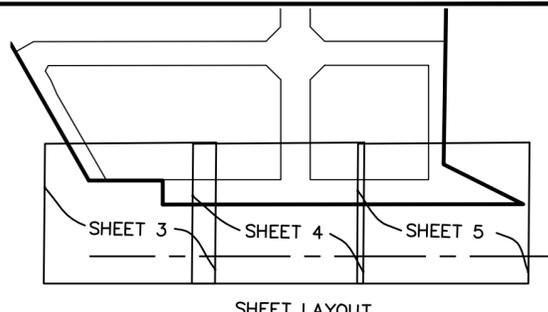
BOUNDARY INFORMATION		
BINDING SITE PLAN - QUENDALL TERMINALS		
CENTURY PACIFIC, L.P.		
CITY OF RENTON, KING COUNTY, STATE OF WASHINGTON		
DRAWN BY LMK/TRS	DATE: 01/11/16	JOB # 2009050.04
CHECKED BY DAB	SCALE: 1" = 100'	SHEET 2 OF 5

SW 1/4 SECTION 29, T24N, R5E, W.M.

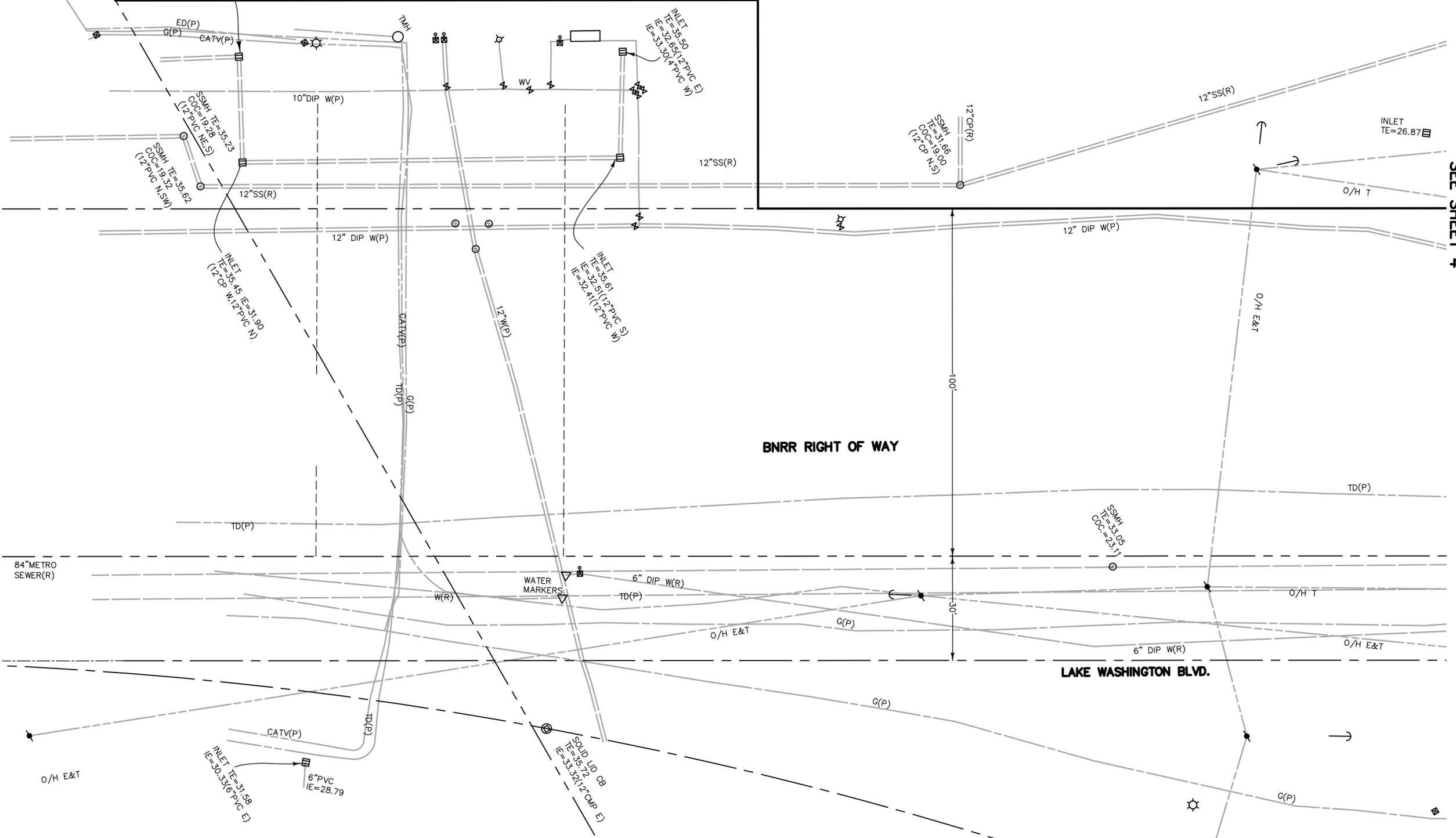
LEGEND

ADS	PLASTIC PIPE	EV	ELECTRICAL VAULT	☒	JUNCTION BOX	TD	TELEPHONE DUCT
CATV	BLOWOFF VALVE	ET	ELECTRICAL TRANSFORMER	O/H E/T	OVERHEAD ELECTRIC/TELEPHONE	■	TELEPHONE SENTRY BOX
COC	CABLE TELEVISION	FIRC	ELECTRIC METER	○	POWER POLE	TV	TELEPHONE VAULT
CONC.	LIGHT POLE	FM	FOUND IRON ROD AND CAP	○	POWER POLE W/ LIGHT	TMH	TELEPHONE MANHOLE
ED	CENTER OF CHANNEL	○	FORCE MAIN	(P)	PAINTED UTILITY LOCATION	TE	TOP ELEVATION
	CONCRETE	○	FIRE HYDRANT	(R)	RECORD UTILITY LOCATION	⊕	WATER METER
	DRAIN INLET	G	GAS VALVE	SS CO	SANITARY SEWER CLEANOUT	W	WATER MANHOLE
	ELECTRICAL DUCT	GM	GAS LINE	SS	SANITARY SEWER	WV	WATER LINE
		→	GAS METER	SSMH (S)	SANITARY SEWER MANHOLE		WATER VAULT
		→	GUY ANCHOR	SD	STORM DRAIN		WATER VALVE
		→	INVERT ELEVATION	SDMH (D)	STORM DRAIN MANHOLE		

DATE OF TOPOGRAPHIC MAPPING
FIELDWORK: JUNE, 2009



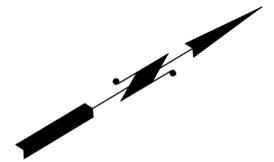
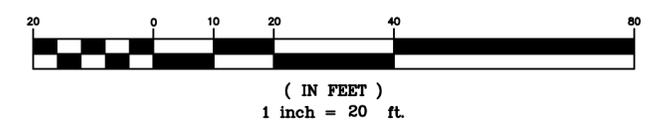
SEE SHEET 4



BNRR RIGHT OF WAY

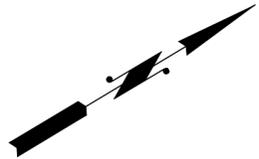
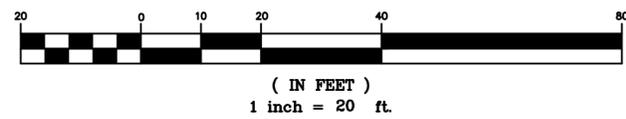
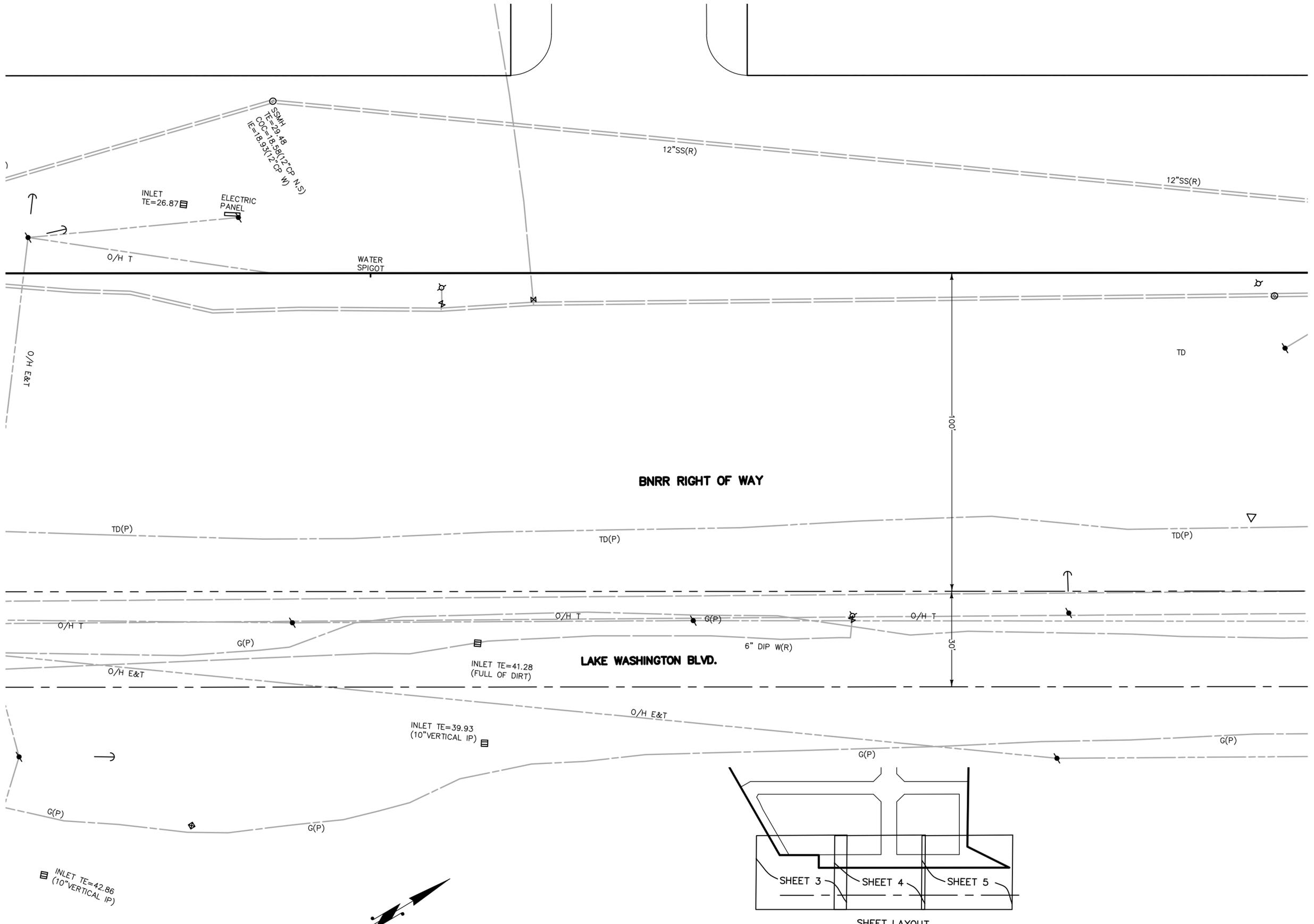
LAKE WASHINGTON BLVD.

EXISTING UTILITIES



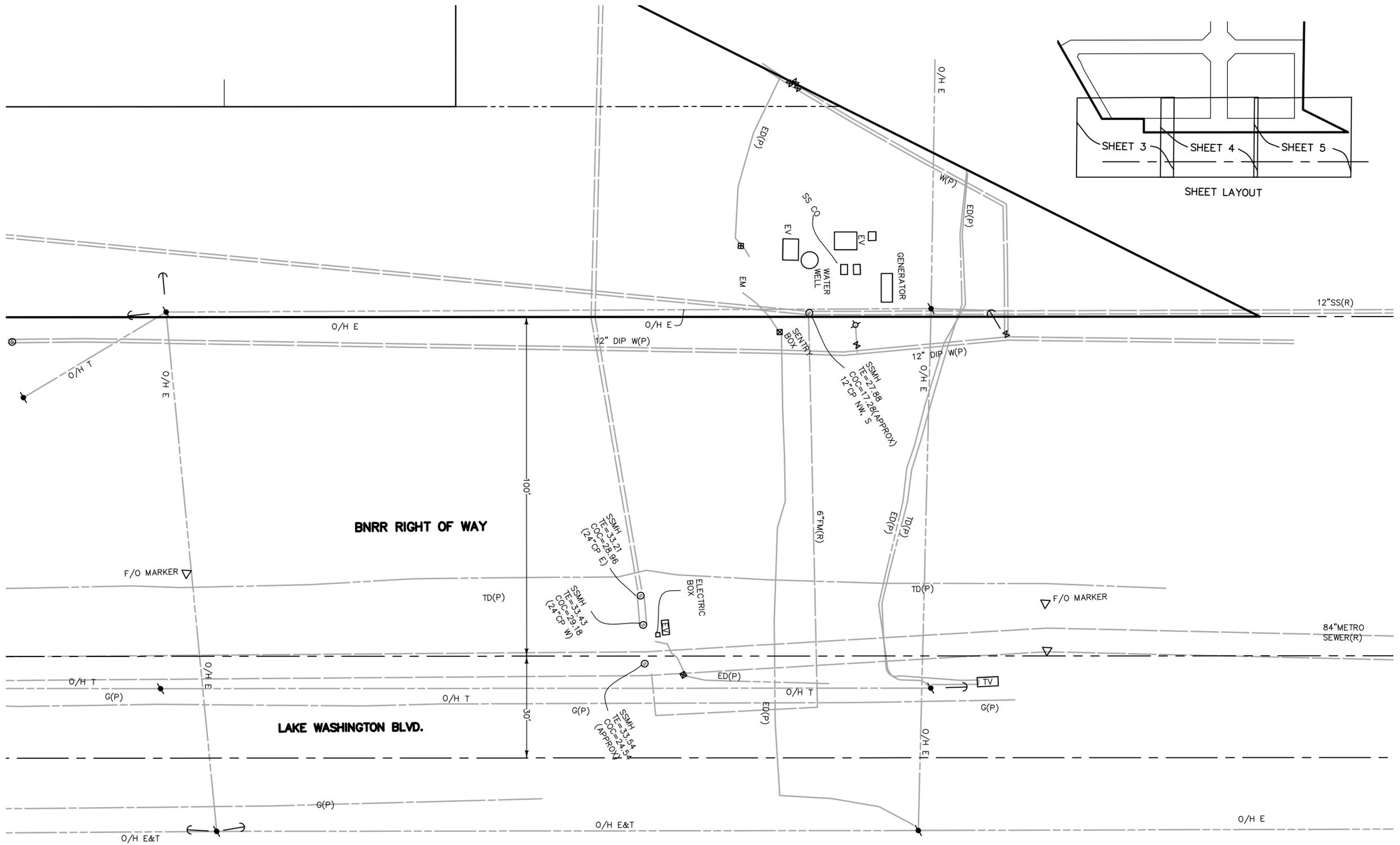
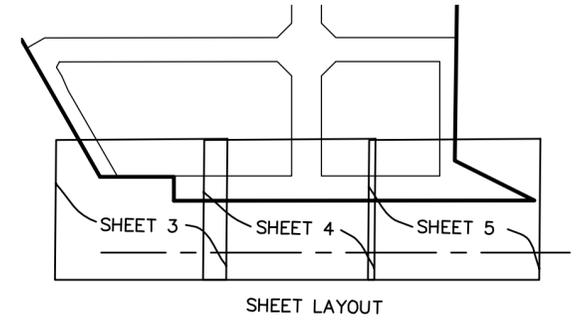
BRH
BUSH, ROED & HITCHINGS, INC.
 CIVIL ENGINEERS & LAND SURVEYORS
 2009 MINOR AVENUE EAST
 SEATTLE, WASHINGTON 98102-3513
 (206) 323-4144
 1-800-935-0508
 FAX# (206) 323-7135

BINDING SITE PLAN - QUENDALL TERMINALS CENTURY PACIFIC, L.P. CITY OF RENTON, KING COUNTY, STATE OF WASHINGTON		
DRAWN BY LMK/TRS	DATE: 01/11/16	JOB # 2009050.04
CHECKED BY DAB	SCALE: 1" = 20'	SHEET 3 OF 5

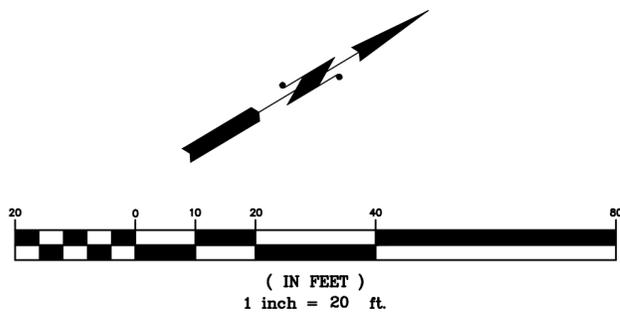


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EXISTING UTILITIES		
BINDING SITE PLAN - QUENDALL TERMINALS		
CENTURY PACIFIC, L.P.		
CITY OF RENTON, KING COUNTY, STATE OF WASHINGTON		
DRAWN BY LMK/TRS	DATE: 01/11/16	JOB # 2009050.04
CHECKED BY DAB	SCALE: 1" = 20'	SHEET 4 OF 5



SEE SHEET 4



BUSH, ROED & HITCHINGS, INC.
 CIVIL ENGINEERS & LAND SURVEYORS

2009 MINOR AVENUE EAST
 SEATTLE, WASHINGTON 98102-3513

(206) 323-4144
 1-800-935-0508
 FAX# (206) 323-7135

EXISTING UTILITIES		
BINDING SITE PLAN - QUENDALL TERMINALS CENTURY PACIFIC, L.P. CITY OF RENTON, KING COUNTY, STATE OF WASHINGTON		
DRAWN BY LMK/TRS	DATE: 01/11/16	JOB # 2009050.04
CHECKED BY DAB	SCALE: 1" = 20'	SHEET 5 OF 5

HILLIS CLARK MARTIN & PETERSON P.S.
1221 Second Avenue, Suite 500
Seattle, WA 98101-2925
Telephone: (206) 623-1745
Fax: (206) 623-7789

Attorneys for Applicant
Century Pacific, L.L.L.P.

CITY OF RENTON

FEB 18 2016

RECEIVED
CITY CLERK'S OFFICE

4:09
p.m.

Hand delivered

CITY OF RENTON
HEARING EXAMINER

In re:

QUENDALL TERMINALS FEIS AND
MITIGATION DOCUMENT, SEPA APPEAL

Project No. LUA09-151

**JOINT STIPULATION AND PROPOSED
ORDER DISMISSING APPEAL**

I. STIPULATION

IT IS HEREBY STIPULATED by and between the parties hereto that the above-entitled cause, having been fully settled and compromised, may be dismissed with prejudice through entry of the subjoined Order of Dismissal, with each party to bear its own costs.

DATED this 18th day of February, 2016.

HILLIS CLARK MARTIN & PETERSON P.S.

By Ann M. Gygi
Amit D. Ranade, WSBA #34878
Ann M. Gygi, WSBA #19912
Attorneys for Applicant
Century Pacific, L.L.L.P.

SOUTH END GIVES BACK

By Brad Nicholson
Its President

Brad Nicholson
BRAD NICHOLSON

II. ORDER OF DISMISSAL

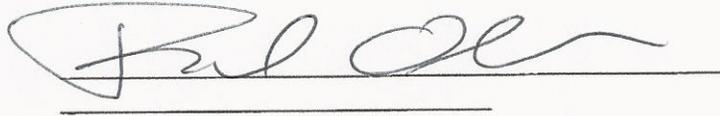
THIS MATTER came before the Renton Hearing Examiner on the foregoing Stipulation of the parties. Based on the foregoing Stipulation, the Renton Hearing Examiner

JOINT STIPULATION AND PROPOSED ORDER DISMISSING
APPEAL WITH PREJUDICE - 1

HILLIS CLARK MARTIN & PETERSON P.S.
1221 Second Avenue, Suite 500
Seattle, WA 98101-2925
Telephone: (206) 623-1745
Fax: (206) 623-7789

hereby ORDERS, ADJUDGES, and DECREES that the appeal of Quendall Terminals Project
LUA09-151 and all claims alleged by the parties are hereby DISMISSED WITH
PREJUDICE, and each party will bear its own costs.

DATED this 22nd day of February, 2016.



Presented by:

HILLIS CLARK MARTIN & PETERSON P.S.

By Ann M. Gygi
Amit D. Ranade, WSBA #34878
Ann M. Gygi, WSBA #19912
Attorneys for Applicant
Century Pacific, L.L.L.P.

SOUTH END GIVES BACK

By [Signature]
Its President

[Signature]
BRAD NICHOLSON

ND: 19958.003 4853-2560-7726v1

JOINT STIPULATION AND PROPOSED ORDER DISMISSING
APPEAL WITH PREJUDICE - 2

HILLIS CLARK MARTIN & PETERSON P.S.
1221 Second Avenue, Suite 500
Seattle, WA 98101-2925
Telephone: (206) 623-1745
Fax: (206) 623-7789

Vanessa Dolbee

From: Phil Olbrechts <olbrechtslaw@gmail.com>
Sent: Monday, February 22, 2016 9:59 PM
To: brad nicholson
Cc: ann.gygi@hcmp.com; cmathewson@centurypacificlp.com; Jason Seth; Vanessa Dolbee; Cynthia Moya; Larry Warren
Subject: Re: Renton - Quendall Homes (LUA-09-151)

Follow Up Flag: Follow up
Flag Status: Flagged

Staff,

This will be the last addition to the email string regarding the FEIS appeal. As requested before, please have five copies ready for the hearing should anyone need to see these emails when I disclose these ex parte contacts with Mr. Nicholson.

Mr. Nicholson,

I would normally not further complicate the record of this case by further communications with you, but it appears that there is some major misunderstanding or miscommunication going on and I want to take one last attempt at rectifying it. I wish I could just talk to you about this, but as the decision maker my ability to communicate with you is very limited due to the reasons identified in my first email to you. Ultimately, however, this will have to be our last communication regarding your appeal unless you plan on making some motion that you entered into the stipulated dismissal order due to some form of fraud or misrepresentation. Any other further information you want me to consider should be sent exclusively to the planning staff as comment on the application.

As I identified in my first email to you, I don't become involved in an appeal until it's time to consider whether a prehearing conference or email exchange is in order. This usually occurs four to six weeks prior to the scheduled appeal date. In this case you would likely have received an email from me to all appeal parties inquiring whether the parties wanted to resolve some prehearing procedural issues or otherwise desired a prehearing order outlining hearing procedures. A request for such a prehearing order is usually initiated by one of the appeal parties, but I will often initiate that inquiry on my own if no one beats me to it.

I will also address any proposed orders or prehearing motions when they come in. Beyond this, planning and city clerk staff are responsible for processing an appeal. The role of City staff and myself does not change because you've persuaded staff to send me your notice of appeal earlier than the completion of the staff report. I'm not sure what type of response you were looking for from your appeal statement. If you just wanted an acknowledgement that your appeal had been filed, then staff would be responsible for that. If they don't issue some sort of acknowledgment as a matter of course, I'm sure they would provide you with something upon request. If you had any questions about how the appeal would be processed or scheduled, all you had to do was ask staff. If you disagreed with how staff was handling some prehearing procedural issue regarding your appeal, you were free to either file a motion with myself ahead of time or to raise the issue at the hearing. If you had made a legally compelling argument that consolidation should not have occurred during your appeal hearing, I would not have had any problem segregating out your appeal (although for future reference, the SEPA rules requiring consolidation are fairly clear and I've yet to come across any argument to the contrary).

If you are upset because I didn't read your appeal months prior to the appeal hearing, there is no reason to be. There's nothing I could have done with any knowledge I would have gained from reading your appeal months in advance. Reading appeal statements too far in advance (especially those exceeding the more typical 10 pages and under) can be a tremendous waste of time since the appeal can easily be narrowed or even withdrawn over time and also because I will have to re-read everything once the hearing date is close. Excluding any prehearing motions or orders that may be presented to me, I only need to know about the details of your appeal in time for the hearing on your appeal. For the stipulated motion to dismiss, I just needed documentation establishing what hearing parties should be included in the order, and I got that information when you pointed out that your notice of appeal had been emailed to me months earlier. If you had not agreed to have your appeal dismissed, I would have read your entire notice of appeal prior to the hearing and I would have gone through it with a fine toothed comb after the hearing as I prepared my decision. It's entirely possible that you would not have liked the result of my decision on your appeal, but I can assure you that you would not have been able to sincerely assert that your issues had not been thoroughly reviewed and addressed.

Once the hearing on Quendall is over and the appeal period has expired I will be happy to discuss this with you further (assuming the discussion doesn't relate to some other pending appeal or application). Also, if it wasn't clear to staff before, it is appropriate for staff to recommend to the parties of an appeal that they request some sort of prehearing conference or email exchange from me if the appeal parties have procedural questions about the conduct of a hearing. For appeal parties represented by attorneys (which has usually been the case), there isn't much confusion about how to participate. For unrepresented citizens, however, I'm sure there's room for improvement as to how to make hearing participants comfortable with the process. Land use appeals in Renton are rare, especially when they involve unrepresented parties.

On Mon, Feb 22, 2016 at 1:48 PM, brad nicholson <brad827@hotmail.com> wrote:
your Honor,

Well I want to apologize but well it took 5 months to get a response? the appeal notice i.e "The facts are dispositive" while it took 24 hours to respond to the PRP's
I am just wondering do you need to have the EPA sign off on the case too? It could be Cami Grandinetti.

Respectfully
Brad Nicholson

From: LWarren@Rentonwa.gov
To: olbrechtslaw@gmail.com; brad827@hotmail.com; CMoya@Rentonwa.gov; VDolbee@Rentonwa.gov; JSeth@Rentonwa.gov; cmathewson@centurypacificlp.com; ann.gygi@hcmp.com
Subject: RE: Renton - Quendall Homes (LUA-09-151)
Date: Mon, 22 Feb 2016 16:38:55 +0000

Mr. Olbrechts, the city has no objection to the stipulated order. If you have any questions, please let me know.

From: Phil Olbrechts [<mailto:olbrechtslaw@gmail.com>]
Sent: Monday, February 22, 2016 7:44 AM
To: brad nicholson; Cynthia Moya; Vanessa Dolbee; Larry Warren; Jason Seth; cmathewson@centurypacificlp.com;

ann.gygi@hcmp.com

Subject: Re: Renton - Quendall Homes (LUA-09-151)

All Appellants,

The proposed stipulated order is missing one of the parties to the appeal. As outlined in RMC 4-8-110(E)(7), the City is a party to the appeal. I will sign the stipulated order if I get email confirmation from the City that it has no objection to the stipulated order.

Please note Mr. Nicholson has sent me another email, included in this email string, that was not cc'd to the other parties to the appeal. As before, I request that staff include this updated email string as one of their hearing exhibits. They should also have five copies of this email string available at the hearing so that I can disclose the ex parte communications with Mr. Nicholson and give the opportunity for the public to review and respond to those communications. In response to Mr. Nicholson's second email, as noted in my last email to him, I received a copy of his notice of appeal (all 200+ pages) as an email attachment from the City Clerk's Office last September. Other than a response from staff as to whether they have an objection to the stipulated order, I ask that I receive no further communications from the parties on the appeal regarding the appeal. For the reasons outlined in my last email to Mr. Nichols, it is important that pre-hearing ex parte communications be limited as much as possible.

On Sun, Feb 21, 2016 at 8:49 PM, brad nicholson <brad827@hotmail.com> wrote:

Your Honor,

I can recall with crystal clarity. 5 weeks after filing the appeal, I called the City Clerk and requested information about why I received no acknowledgement of its receipt or correspondence whatsoever. It would appear that I have been involved in as many appeals in Renton as you have and in the past, the normal process has been to give a short explanation of what is going on. At first the Clerk told me that he had posted it on the web and that I would need to talk to Staff about it. I had complained to him that I had spent considerable time and had addressed the appeal to you and not to staff. My comment letter was addressed to staff but my appeal notice along with \$250.00 was addressed to you according to staff instructions. The Clerk then assured me in no uncertain terms that the appeal document went directly to you and that you had a copy of it in your possession. I complained to him that I paid the fee for the appeal to go to you and not to staff. Thus I am surprised that you would indicate that you have no documents. I am also surprised that you did not get it and it was never in your possession. I am just saying that I had also been quite disturbed that staff had been the ones to decide on the appeal consolidation even though I think you are correct in presuming that it may have been generally known to be the case. But I had not understood that they had jurisdiction to decide issues of Law in a pending appeal.

Respectfully

Brad Nicholson

Date: Sun, 21 Feb 2016 06:07:51 -0800

Subject: Re: Renton - Quendall Homes (LUA-09-151)

From: olbrechtslaw@gmail.com

To: brad827@hotmail.com; CMoya@rentonwa.gov; VDolbee@rentonwa.gov; LWarren@rentonwa.gov; JSeth@rentonwa.gov; cmathewson@centurypacificlp.com; ann.gygi@hcmp.com

Staff,

Please include this email string in the exhibits to the staff report on the Quendall Homes application.

Mr. Nicholson,

Thank you for the clarification of Quendall Homes. I will likely send out a signed order tomorrow once I've had an opportunity to review the filed notice of appeal.

From your assertions that I may have "forgotten" information that you apparently believe I at one point knew or should have known about your project, it appears you might have some misunderstanding about my level of involvement in your case prior to hearing. By a combination of law and necessity, I'm essentially required to know as little about your project as possible until staff has completed its staff report and list of proposed hearing exhibits. By state statute and numerous court opinions I am not allowed to engage in conversations with staff, the applicant or any hearing party about the substantive merits of your case outside the hearing process. Because of these legal requirements, you can be assured that all of my knowledge about your case is strictly limited to the testimony presented during the project hearing and the exhibits admitted into evidence. This puts you on equal footing with all other hearing participants and ensures that you have the opportunity to address any evidence presented to me that you may believe to be inaccurate or incomplete.

Up until today my knowledge of your case had been limited to knowing it's big, controversial and involves a superfund site. The only reason I knew this much is because every few weeks I ask the planning manager if anything big is coming up, so that I may plan my schedule and workload in advance. Land use appeals (as opposed to applications) sometimes involve a little more advance information due to the necessity to sometimes hold prehearing conferences. Prehearing conferences are prehearing meetings or email exchanges with appeal hearing participants designed to address procedural issues in advance in order to provide for a more efficient appeal hearing. Issues usually held at prehearing conferences typically include setting limits on the time for testimony, requiring the parties to identify witnesses and exhibits, and setting deadlines for prehearing motions and briefing. If no party has specifically requested a prehearing conference, I will typically ask for a copy of

the notice of appeal four to six weeks in advance of a scheduled appeal hearing to determine whether a prehearing conference would be useful.

As best as I can recall, Renton has only had a couple land use appeal hearings in the last five years that I've worked with the city. Renton is unique amongst my thirteen hearing examiner clients in that it apparently regularly sends me a copy of the appeal statement before I ask for it and before the staff report is complete. There's nothing wrong with that practice, since it's fairly clear that the appeal statement will be included in the staff's exhibit list once the staff report is completed. However, it's not going to attract my notice until I get close to the appeal hearing and start considering the merits of a prehearing conference. From your email response yesterday I was able to find an email from the City Clerk's office from last September that contained an attachment of your appeal statement. That should give me the information I need to ensure that the stipulated order includes all necessary parties.

Your email also asserts that I've forgotten about a consolidation order. It's fairly undisputed that state law requires an appeal of FEIS adequacy to be consolidated with the hearing on its underlying permit application. I would be surprised if staff found it necessary to ask me about consolidation. Also, as previously discussed, the circumstances under which staff can talk to me about a case are very limited outside the hearing process. I don't recall making any comments to staff regarding consolidation but if I did you are correct that I have forgotten about it.

On Sun, Feb 21, 2016 at 12:30 AM, brad nicholson <brad827@hotmail.com> wrote:

Your Honor,

You may be looking for "Quendall Homes" according the previous email. The documents concern "Quendall Terminals" EIS decision which you decided to consolidate with the Master plan hearing around 5 months ago. Vanessa Dolbee informed me that is was because of "State Law" You may have forgotten about it but the appeal documents were sent directly to you from the clerk Jason Seth. You may have also forgotten it was an appeal that was 288 pages against the adequacy of review where you be approving the Master Plan for around 700 houses next to the Seahawks training facility on the superfund site that will undergo cleanup after you take your actions?

Respectfully,
Brad Nicholson

Date: Sat, 20 Feb 2016 08:32:57 -0800
Subject: Re: Renton - Quendall Homes (LUA-09-151)
From: olbrechtslaw@gmail.com

To: CMoya@rentonwa.gov

CC: VDolbee@rentonwa.gov; LWarren@rentonwa.gov; JSeth@rentonwa.gov;
cmathewson@centurypacificlp.com; brad827@hotmail.com; ann.gygi@hcmp.com

Please confirm that all parties to the SEPA appeal have signed the stipulated order to dismiss. I have no documentation on the appeal, so I have no knowledge of what parties are involved.

On Thu, Feb 18, 2016 at 4:34 PM, Cynthia Moya <CMoya@rentonwa.gov> wrote:

Mr. Olbrechts,

We have just received a Joint Stipulation & Proposed Order Dismissing Appeal in the Quendall Terminals FEIS & Mitigation Document, SEPA Appeal (File #LUA-09-151). The parties have asked that you sign the attached document as soon as possible.

If you have any questions, please feel free to call Vanessa at [425-430-7314](tel:425-430-7314).

Thank you,

Cindy Moya, Records Management Specialist
City of Renton - Administrative Services/City Clerk Division
cmoya@rentonwa.gov
[425-430-6513](tel:425-430-6513)



NOTE:
APPROXIMATELY 1.8 ACRE OF INDOOR AND/OR
OUTDOOR AREA FOR ACTIVE RECREATION SHALL
BE DEVELOPED ON THE PLAZA/PATIO DECKS
DURING FINAL DESIGN DEVELOPMENT

POST DEVELOPMENT ACREAGES

BUILT AREA (IMPERVIOUS AREA)

BUILDING AREAS =
 BUILDING AREAS = 187,350sf
 PARKING DECK AREAS = 59,000sf
 PARKING DECK LANDSCAPE AREAS = 25,000sf
 COURTYARD PLAZAS = 117,600sf
 COURTYARD MISC. DECKS = 4,026sf
 TOTAL BUILDING AREAS = 392,976sf

PAVED AREAS (R.O.W., ROADS, PEDESTRIAN/BIKE PATHS) =
 STREET 'A', 'B' & 'C' AREAS = 99,250sf
 STREETS 'D' & 'E' AREAS = 23,522sf
 SIDEWALK AREA = 60,800sf
 TOTAL PAVED AREAS = 183,572sf

SURFACE PARKING AREAS = 89,000sf
 TOTAL IMPERVIOUS AREAS = 665,548sf

PERVIOUS AREA

DESIGNATED NATURAL/OPEN SPACE AREA = 140,338sf
 UNPAVED FIRE LANE/PEDESTRIAN TRAIL = 19,970sf
 STREET LEVEL LANDSCAPED AREAS = 15,300sf
 OTHER LANDSCAPED AREAS = 33,495sf
 LOT 7 SATELLITE PROPERTY = 50,725sf
 TOTAL PERVIOUS AREAS = 259,828sf

UNIT SUMMARY

SW RESIDENTIAL TOTAL UNITS = 257
 NW RESIDENTIAL TOTAL UNITS = 127
 SE RESIDENTIAL TOTAL UNITS = 154
 NE RESIDENTIAL TOTAL UNITS = 154
 TOTAL UNITS = 692

DECK PARKING:
 SE QUADRANT = 130 DECK PARKING STALLS
 NE QUADRANT = 39 DECK PARKING STALLS
 TOTAL DECK PARKING = 169 STALLS

LEGEND

- DUMPS/ RECYCLE BIN
- UTILITY AND ELECTRICAL SPACE
- EXIT STAIR
- LOBBY AREA

LEGEND

- OHWM
- 50' WETLAND SETBACK
- 100' OHWM SETBACK
- WETLAND EDGE

SITE & BUILDING SUMMARY

BUILDING CODE: 2009 IBC
 SITE ZONING: COR
 OCCUPANCY TYPE: M & R-2
 CONSTRUCTION TYPE: I & VA - SPRINKLERED
 MAX. BUILDING HEIGHT: 10 STORIES OR 125'

TOTAL RESIDENTIAL UNITS AREA

SW 1 RESIDENTIAL (5 FLOORS & 71 UNITS) = 78,100sf
 SW 2 RESIDENTIAL (5 FLOORS & 80 UNITS) = 88,000sf
 SW 3 RESIDENTIAL (4 FLOORS & 64 UNITS) = 70,400sf
 SW 4 RESIDENTIAL (3 FLOORS & 42 UNITS) = 78,100sf
 NW 1 RESIDENTIAL (5 FLOORS & 71 UNITS) = 46,200sf
 NW 2 RESIDENTIAL (4 FLOORS & 56 UNITS) = 61,600sf
 SE 1 RESIDENTIAL (5 FLOORS & 82 UNITS) = 78,100sf
 SE 2 RESIDENTIAL (4 FLOORS & 72 UNITS) = 79,200sf
 NE 1 RESIDENTIAL (5 FLOORS & 82 UNITS) = 78,100sf
 NE 2 RESIDENTIAL (4 FLOORS & 72 UNITS) = 79,200sf
 TOTAL BUILDING AREAS = 737,000sf

TOTAL RESTAURANT AREA = 9,000sf

TOTAL RETAIL AREA = 20,025sf

TOTAL PARKING AREA

SW LOWER PARKING LEVEL = 125,180sf
 NW LOWER PARKING LEVEL = 41,800sf
 SE LOWER PARKING LEVEL = 114,870sf
 NE LOWER PARKING LEVEL = 75,490sf
 TOTAL LOWER PARKING LEVEL AREAS = 357,340sf

TOTAL ENCLOSED AREA = 1,123,365sf

STRUCTURAL RESIDENTIAL COURTYARDS = 117,600sf

STRUCTURAL OPEN PARKING DECKS = 84,000sf

TOTAL AREA = 1,324,965sf

LAND AREAS

GROSS SITE AREA = 925,376sf
 NATURAL PUBLIC OPEN SPACES:
 NATURAL AREAS ALONG SHORELINE TRAIL = 140,338sf
 SHORELINE FIRE LANE/PEDESTRIAN TRAIL 19,970sf
 SUB TOTAL = 160,308sf

OTHER AREAS:

STREET LEVEL = 122,772sf (EXCLUDES SIDEWALKS & LANDSCAPE)

LANDSCAPED COURTYARDS = 117,600sf

SIDEWALKS:

SIDEWALKS IN PUBLIC R.O.W. = 36,800sf

SIDEWALKS NOT IN PUBLIC R.O.W. = 24,000sf

PAVED PARKING AREAS:

DECK PARKING AREA = 84,000sf (INCLUDES SIDEWALKS & LANDSCAPE)

SURFACE PARKING AREA = 89,000

COURTYARD MISC. DECKS = 4,026sf

LANDSCAPE AREAS:

STREET LEVEL LANDSCAPE = 15,300sf

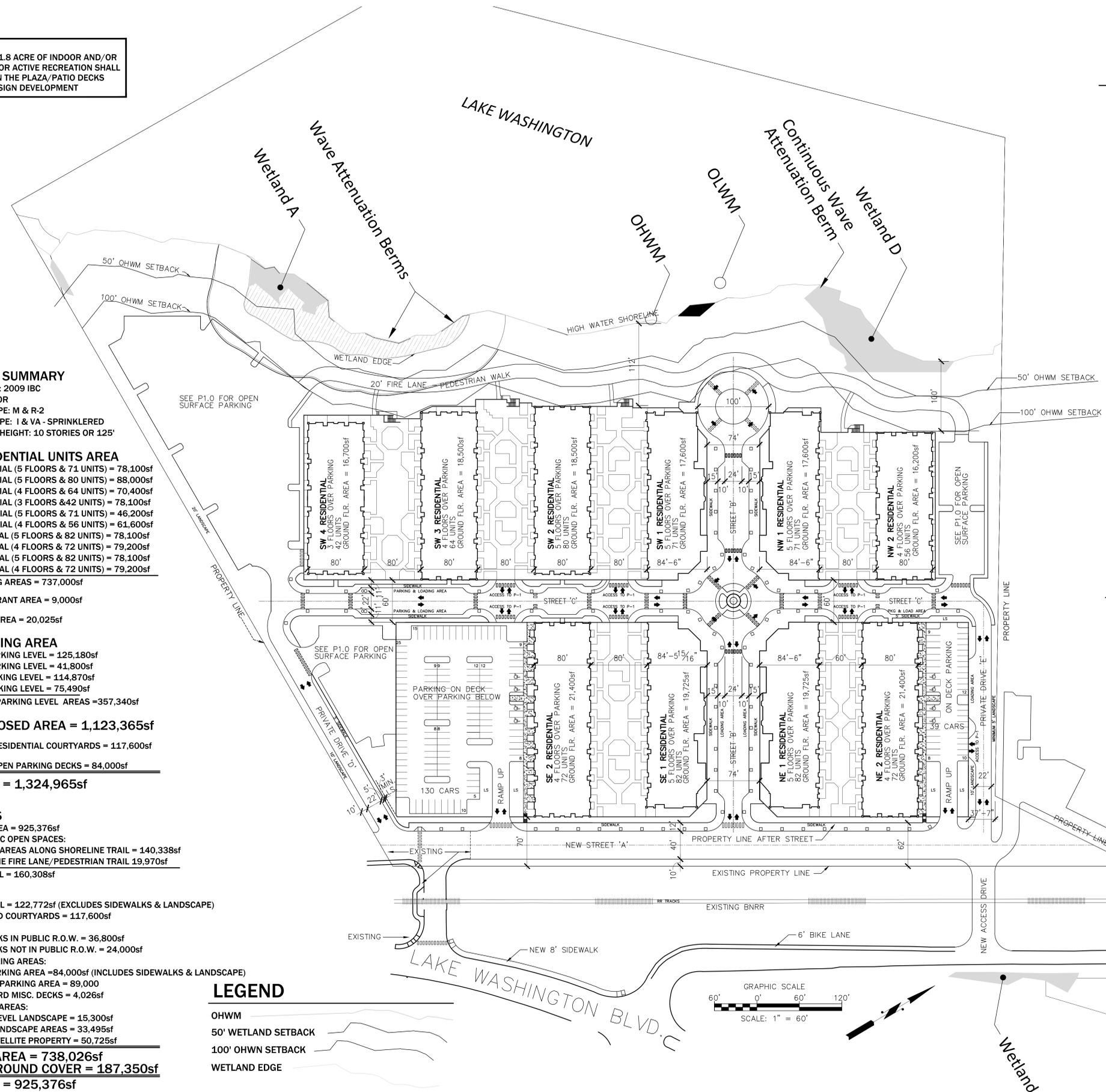
OTHER LANDSCAPE AREAS = 33,495sf

LOT 7 SATELLITE PROPERTY = 50,725sf

SUB TOTAL AREA = 738,026sf

BUILDING GROUND COVER = 187,350sf

TOTAL AREA = 925,376sf



QUENDALL TERMINALS - PREFERRED ALTERNATIVE

RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP
 EXHIBIT 7

SITE PLAN - PREFERRED ALT. 1" = 60'-0"
 LANCE MUELLER & ASSOCIATES
 ARCHITECTS AIA
 130 LAKESIDE • SEATTLE, WA 98122 • 206 325 2553
 sheet
 P0.0

job no.	no.	revision	date
08-109	1		
PKT.	2		
2004 REGISTERED ARCHITECT STATE OF WASHINGTON	3		
12-30-15	4		
6-7-12	5		
4-2-12	6		
4-2-12	7		
4-16-12	8		
3-1-12	9		



LEGEND

- OHWM
- 50' WETLAND SETBACK
- 100' OHWM SETBACK
- WETLAND EDGE

UNIT SUMMARY

SW RESIDENTIAL TOTAL UNITS = 257
 NW RESIDENTIAL TOTAL UNITS = 127
 SE RESIDENTIAL TOTAL UNITS = 154
 NE RESIDENTIAL TOTAL UNITS = 154
TOTAL UNITS = 692

DECK PARKING:
 SE QUADRANT = 130 DECK PARKING STALLS
 NE QUADRANT = 39 DECK PARKING STALLS
TOTAL DECK PARKING = 169 STALLS

LEGEND

- DUMPER / RECYCLE BIN
- UTILITY AND ELECTRICAL SPACE
- EXIT STAIR
- LOBBY AREA

QUENDALL TERMINALS - PREFERRED ALTERNATIVE

RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP

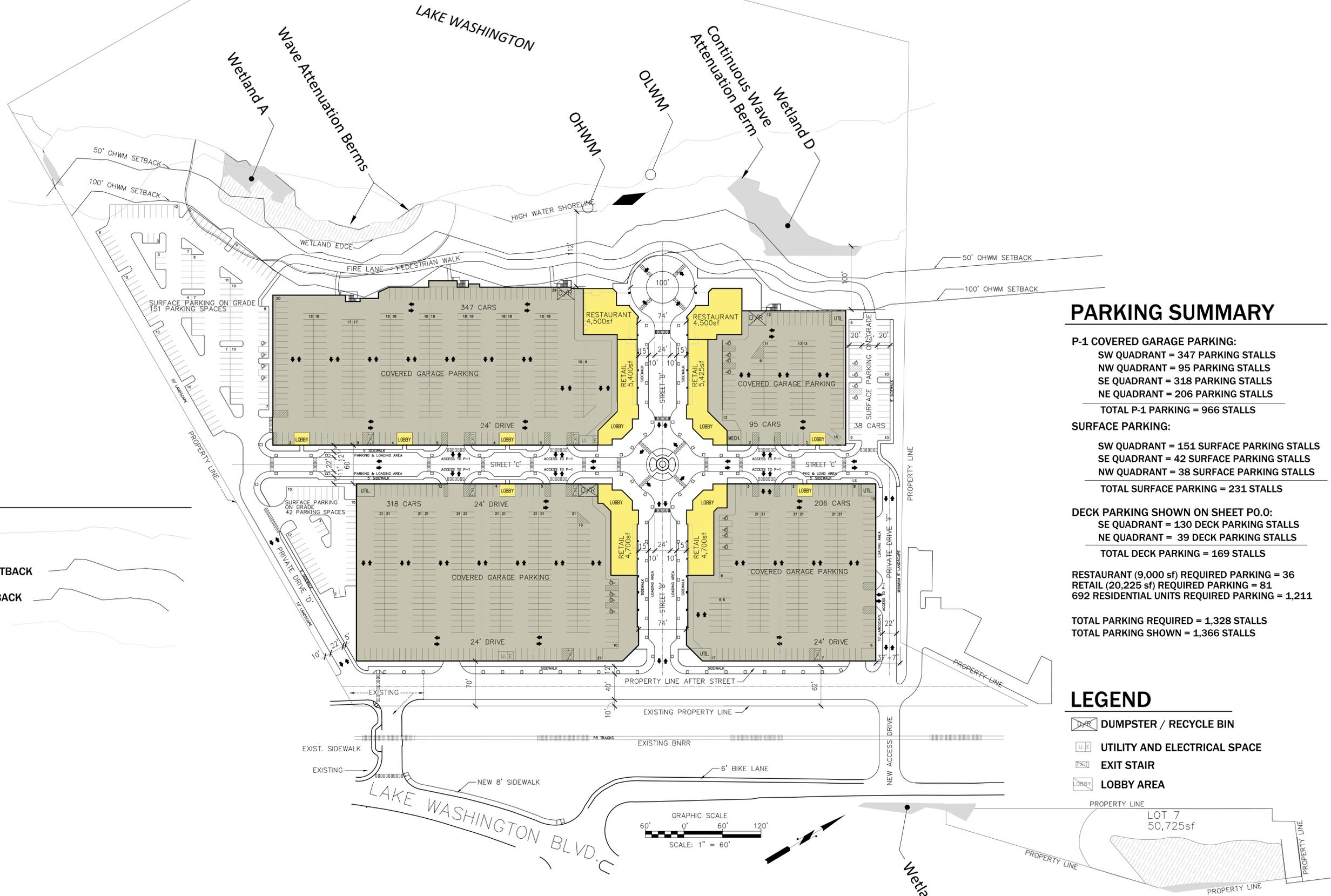
job no.	date	revision	no.	date	revision
09-120	11-16-09	checked	1	3-14-12	
		drawn	2	4-16-12	
			3	4-25-12	
			4	5-2-12	
			5	5-12-12	
			6	12-30-15	

QUENDALL TERMINALS
 RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP

SITE PLAN - PREFERRED ALT. 1" = 60'-0"

LANCE MUELLER & ASSOCIATES
 ARCHITECTS · AIA
 130 LAKESIDE · SEATTLE, WA 98122 · 206.325.2553

sheet
P0.0



LEGEND

- OHWM
- 50' WETLAND SETBACK
- 100' OHWN SETBACK
- WETLAND EDGE

PARKING SUMMARY

P-1 COVERED GARAGE PARKING:
 SW QUADRANT = 347 PARKING STALLS
 NW QUADRANT = 95 PARKING STALLS
 SE QUADRANT = 318 PARKING STALLS
 NE QUADRANT = 206 PARKING STALLS
TOTAL P-1 PARKING = 966 STALLS

SURFACE PARKING:
 SW QUADRANT = 151 SURFACE PARKING STALLS
 SE QUADRANT = 42 SURFACE PARKING STALLS
 NW QUADRANT = 38 SURFACE PARKING STALLS
TOTAL SURFACE PARKING = 231 STALLS

DECK PARKING SHOWN ON SHEET P0.0:
 SE QUADRANT = 130 DECK PARKING STALLS
 NE QUADRANT = 39 DECK PARKING STALLS
TOTAL DECK PARKING = 169 STALLS

RESTAURANT (9,000 sf) REQUIRED PARKING = 36
RETAIL (20,225 sf) REQUIRED PARKING = 81
692 RESIDENTIAL UNITS REQUIRED PARKING = 1,211

TOTAL PARKING REQUIRED = 1,328 STALLS
TOTAL PARKING SHOWN = 1,366 STALLS

LEGEND

- DUMPSTER / RECYCLE BIN
- UTILITY AND ELECTRICAL SPACE
- EXIT STAIR
- LOBBY AREA

QUENDALL TERMINALS - PREFERRED ALTERNATIVE

RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP

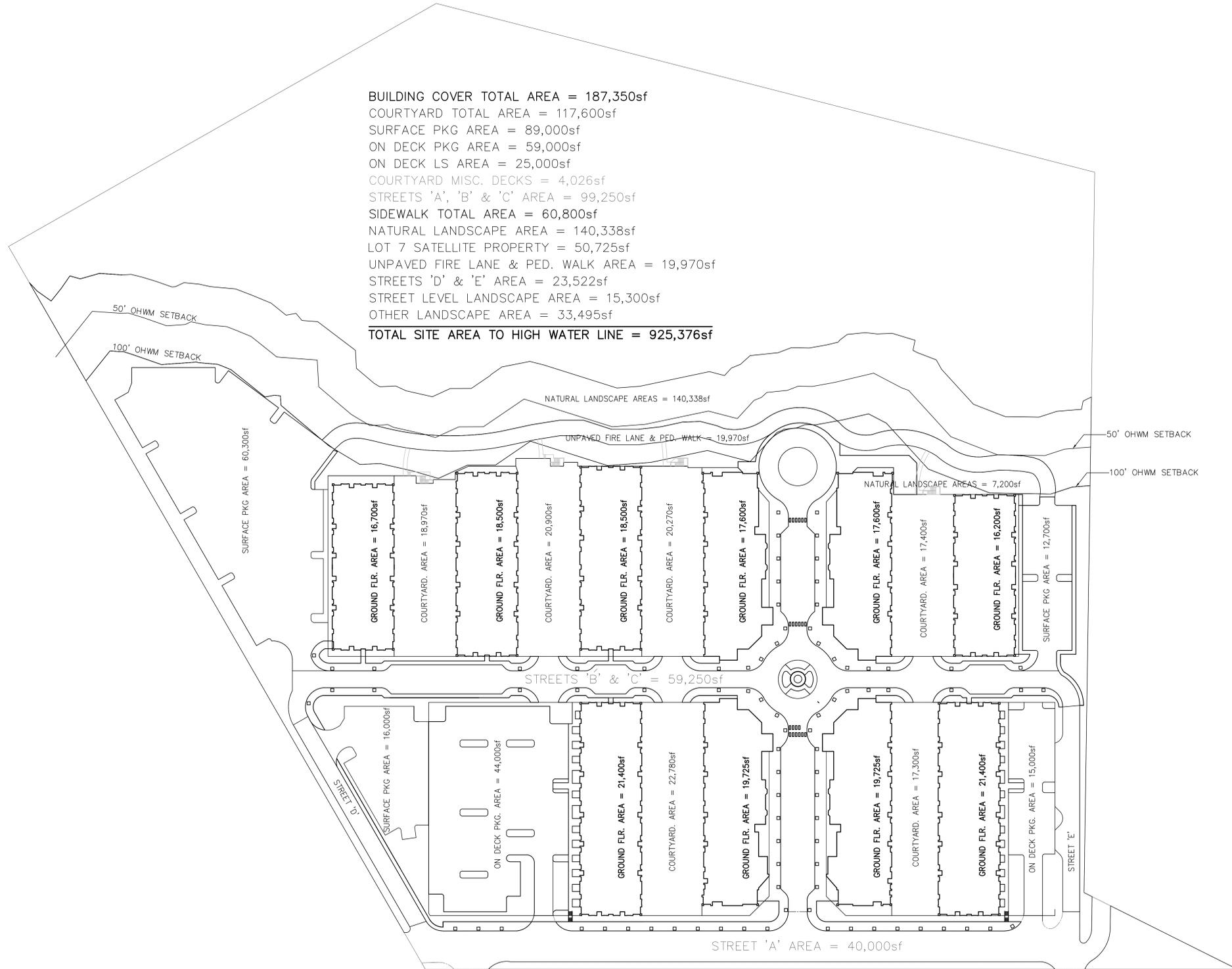
EXHIBIT 8

job no.	no.	revision	date
09-120	6	REVISED PER MITIGATION RECOMM.	12-30-15
	5	PREFERRED ALTERNATIVE REVISED	6-7-12
	4	PREFERRED ALTERNATIVE	5-7-12
	3	ORCA 4/18/12	4-18-12
	2	ORCA 4/18/12	4-18-12
	1	ALTERNATE #6	3-14-12

REGISTERED ARCHITECT
 STATE OF WASHINGTON
 No. 2786

QUENDALL TERMINALS
 RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP

P-1 PARK. LEVEL - PREFERRED ALT. 1" = 60'-0"
 LANCE MUELLER & ASSOCIATES
 ARCHITECTS . A . I . A
 130 LAKESIDE . SEATTLE, WA 98122 . 206 325 2553
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P1.0



BUILDING COVER TOTAL AREA = 187,350sf
 COURTYARD TOTAL AREA = 117,600sf
 SURFACE PKG AREA = 89,000sf
 ON DECK PKG AREA = 59,000sf
 ON DECK LS AREA = 25,000sf
 COURTYARD MISC. DECKS = 4,026sf
 STREETS 'A', 'B' & 'C' AREA = 99,250sf
 SIDEWALK TOTAL AREA = 60,800sf
 NATURAL LANDSCAPE AREA = 140,338sf
 LOT 7 SATELLITE PROPERTY = 50,725sf
 UNPAVED FIRE LANE & PED. WALK AREA = 19,970sf
 STREETS 'D' & 'E' AREA = 23,522sf
 STREET LEVEL LANDSCAPE AREA = 15,300sf
 OTHER LANDSCAPE AREA = 33,495sf
TOTAL SITE AREA TO HIGH WATER LINE = 925,376sf

ON DECK LS AREA = 25,000sf



EXHIBIT 9

AREA OUTLINE OF SPACES	1" = 60'-0"	REGISTERED PROFESSIONAL ARCHITECT	109-120	6	REVISED PER MITIGATION REMOVS.	12-30-15	no.	revision	date
LANGE MUELLER & ASSOCIATES		ARCHITECTS · AIA	PTK	5	PREFERRED ALTERNATIVE	6-7-12	no.	revision	date
			drawn	4	PREFERRED ALTERNATIVE	5-2-12	no.	revision	date
			checked	3	DRAFT	4-2-12	no.	revision	date
				2	DRAFT	4/16/12	no.	revision	date
				1	ALTERNATIVE #6	3-14-12	no.	revision	date

sheet
P2.0

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QUENDALL TERMINALS
RENTON, WASHINGTON
CENTURY PACIFIC, LLLP

QUENDALL TERMINALS - PREFERRED ALTERNATE

RENTON, WASHINGTON
CENTURY PACIFIC, LP

Exterior Design Goals:

Provide a coordinated design language with a variety of details and materials to provide a human scale, highlight portal elements, and provide a visually interesting street-scape and facade.

Retail and Restaurant Space Features:

- A sidewalk orientation with architecturally articulated punched openings with glass extending to sidewalk level.
- Include canopies of steel and glass for weather protection
- Include sconce and canopy lighting fixtures for an appropriate lighting level.
- Street lighting on public right-of-ways and intersections.
- Alternate facade materials and details are used for variety.

To visually conceal the structured parking from street:

- We are providing retail/restaurant at some street facades which completely screens parking behind.

Facades with parking directly behind have the following:

- Architecturally articulated punched openings in-filled with grillwork that also allows climbing vines.
- Intermittent trellis elements are vine covered.
 - Berm and extend landscaping to sill of punched openings.
 - Alternate facade materials and details are used for variety.
 - We avoid solid walls.

Residential Floors Features:

- These floors are setback from the base facade for modulation and visual interest.
- Additional architectural facade modulation is provided via:
 - Horizontal plan modulation with projecting decks.
 - Projecting vertical elements beyond the parapet.
 - Alternate facade materials and details are used for variety.
 - Architectural facade element variety.
- Strong corner elements to highlight the entry portal.

Amenities:

- Large landscape courtyard at level 1 visually screens structured parking below and provides restful areas to view or for sitting, reading, and strolling. It may include an exercise room or entertainment space.
- Potential for rooftop plaza with landscaping and green roof elements.

Buildings:

- There are 4 to 6 levels in these mixed use buildings as follows:
- P1 includes screened structured parking with some retail & restaurant uses south facing in NW1.
 - Floors above P1 are all residential use. Floor 1 includes extensive elevated landscaped courtyards as an amenity.

Exterior Finish:

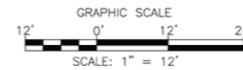
Descriptions:	Material/Finish:
Coping:	Painted Metal, color to match siding.
Glass & store front system:	Green tint insulated glass in aluminum frames, clear anodized - typical. Use clear glass at retail.
Walls at Base of Building (Level P1):	Includes a mix of materials for variety: <ul style="list-style-type: none"> • Concrete - stained or painted finish. • Brick veneer with random color
Walls above Base of Building (Residential Floors 1 through 5):	Includes a mix of materials for variety: <ul style="list-style-type: none"> • Metal Panel Siding - painted finish. • Stucco - painted finish. • Composite Panel Siding - painted finish. • Brick veneer with random color.
Walls at Stair Penthouse & Roof Equipment:	Metal Panel Siding - painted finish.
Railings, Grillwork and Trellis:	Painted Metal, accent colors TBD.
Entry Canopy:	Painted Metal, accent colors TBD. Green tint glazing in aluminum frame.
Sconce Lighting Fixtures:	Typical at Level P1, Selection TBD.



BUILDING NW 2

BUILDING NW 1

PARTIAL WEST ELEVATION FROM LAKE WASHINGTON



BUILDING NW 2

BUILDING NW 1

BUILDING SW 1

BUILDING SW 2

BUILDING SW 3

BUILDING SW 4

OVERALL WEST ELEVATION FROM LAKE WASHINGTON

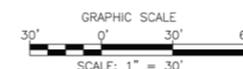


EXHIBIT 10

no.	revision	date
6	REVISED PERMIT APPLICATIONS	12-20-15
5	PREFERRED ALTERNATE	6-7-12
4	PREFERRED ALTERNATE	5-7-12
3	START 4/18/12	4-18-12
2	ALTERNATE #5	3-14-12
1	ALTERNATE #6	3-14-12

09-120
Job no.
PK
drawn
checked
11-16-09
date

REGISTERED ARCHITECT
STATE OF WASHINGTON
CENTURY PACIFIC, LP

QUENDALL TERMINALS
 RENTON, WASHINGTON
 CENTURY PACIFIC, LP

ELEVATIONS - PREFERRED ALT.
 AS NOTED
 LANCE MUELLER & ASSOCIATES
 ARCHITECTS, AIA
 130 LAKESIDE • SEATTLE, WA 98122 • 206 325 2553
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P3.0

11-10-09 N:\DINGS\09120\QUENDALL\P1-0

QUENDALL TERMINALS - PREFERRED ALTERNATE

RENTON, WASHINGTON
CENTURY PACIFIC, LP

Exterior Design Goals:

Provide a coordinated design language with a variety of details and materials to Provide a human scale, highlight portal elements, and provide a visually interesting street-scape and facade.

Retail and Restaurant Space Features:

- A sidewalk orientation with architecturally articulated punched openings with glass extending to sidewalk level.
- Include canopies of steel and glass for weather protection
- Include sconce and canopy lighting fixtures for an appropriate lighting level.
- Street lighting on public right-of-ways and intersections.
- Alternate facade materials and details are used for variety.

To visually conceal the structured parking from street:

- We are providing retail/restaurant at some street facades which completely screens parking behind.

Facades with parking directly behind have the following:

- Architecturally articulated punched openings in-filled with grillwork that also allows climbing vines.
- Intermittent trellis elements are vine covered.
 - Berm and extend landscaping to sill of punched openings.
 - Alternate facade materials and details are used for variety.
 - We avoid solid walls.

Residential Floors Features:

- These floors are setback from the base facade for modulation and visual interest.
- Additional architectural facade modulation is provided via:
 - Horizontal plan modulation with projecting decks.
 - Projecting vertical elements beyond the parapet.
 - Alternate facade materials and details are used for variety.
 - Architectural facade element variety.
- Strong corner elements to highlight the entry portal.

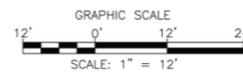
Amenities:

- Large landscape courtyard at level 1 visually screens structured parking below and provides restful areas to view or for sitting, reading, and strolling. It may include an exercise room or entertainment space.
- Potential for rooftop plaza with landscaping and green roof elements.



BUILDING NW 1

PARTIAL SOUTH ELEVATION



Buildings:

There are 4 to 6 levels in these mixed use buildings as follows:

- P1 includes screened structured parking with some retail & restaurant uses south facing in NW1.
- Floors above P1 are all residential use. Floor 1 includes extensive elevated landscaped courtyards as an amenity.

Exterior Finish:

Descriptions:

Coping:

Glass & store front system:

Walls at Base of Building (Level P1):

Walls above Base of Building (Residential Floors 1 through 5)

Walls at Stair Penthouse & Roof Equipment:

Railings, Grillwork and Trellis:

Entry Canopy:

Sconce Lighting Fixtures:

Material/Finish:

Painted Metal, color to match siding.

Green tint insulated glass in aluminum frames, clear anodized - typical. Use clear glass at retail.

Includes a mix of materials for variety:

- Concrete - stained or painted finish.
- Brick veneer with random color

Includes a mix of materials for variety:

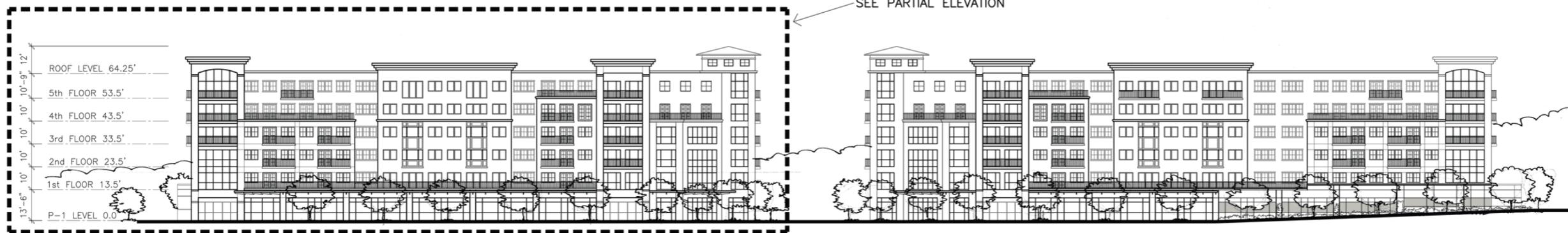
- Metal Panel Siding - painted finish.
- Stucco - painted finish.
- Composite Panel Siding - painted finish.
- Brick veneer with random color.

Metal Panel Siding - painted finish.

Painted Metal, accent colors TBD.

Painted Metal, accent colors TBD. Green tint glazing in aluminum frame.

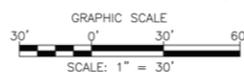
Typical at Level P1, Selection TBD.



BUILDING NW 1

BUILDING NE 1

SOUTH ELEVATION



QUENDALL TERMINALS
 RENTON, WASHINGTON
 CENTURY PACIFIC, LP

AS NOTED

ELEVATIONS - PREFERRED ALT.
 LANCE MUELLER & ASSOCIATES
 ARCHITECTS AIA
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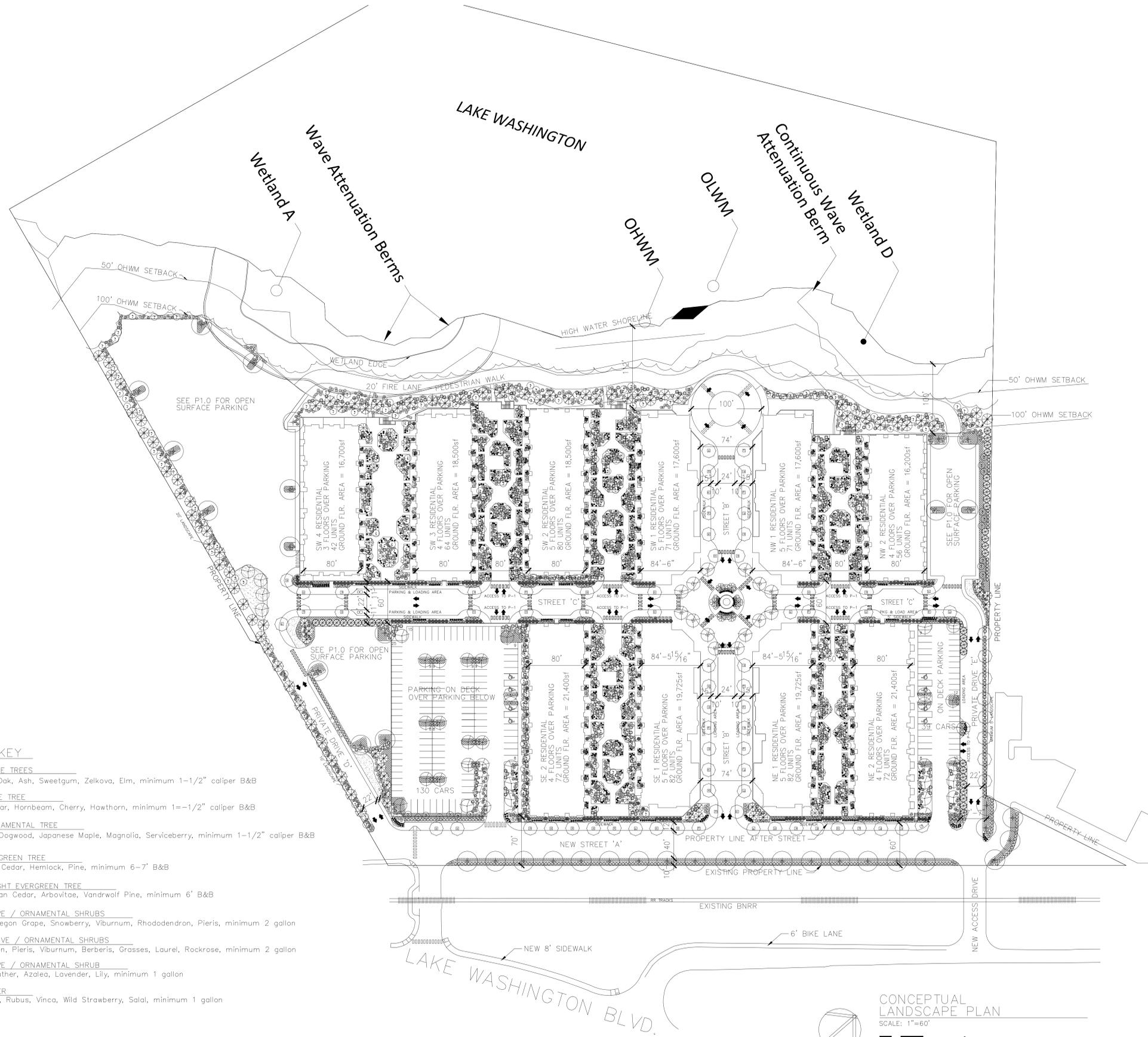
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no.	revision	date
6	REVISED PERMITTING REQUIREMENTS	12-30-15
5	PREFERRED ALTERNATE	6-7-12
4	PREFERRED ALTERNATE	4-25-12
3	DRAFT 4/25/12	4-25-12
2	DRAFT 4/16/12	4-16-12
1	ALTERNATE #0	3-14-12

09-120 Job no.
PKY
drawn
checked
11-16-09
date

REGISTERED
LANCE MUELLER
DATE OF REGISTRATION

N:\DWG5\09120\QUENDALL\P1-0



PLANT KEY

- LARGE SHADE TREES**
Red Maple, Oak, Ash, Sweetgum, Zelkova, Elm, minimum 1-1/2" caliper B&B
- SMALL STREET TREE**
Flowering Pear, Hornbeam, Cherry, Hawthorn, minimum 1-1/2" caliper B&B
- ACCENT/ORNAMENTAL TREE**
Vine Maple, Dogwood, Japanese Maple, Magnolia, Serviceberry, minimum 1-1/2" caliper B&B
- LARGE EVERGREEN TREE**
Douglas Fir, Cedar, Hemlock, Pine, minimum 6-7' B&B
- SMALL UPRIGHT EVERGREEN TREE**
Juniper, Hagan Cedar, Arbovitae, Vandrolf Pine, minimum 6" B&B
- LARGE NATIVE / ORNAMENTAL SHRUBS**
Dogwood, Oregon Grape, Snowberry, Viburnum, Rhododendron, Pieris, minimum 2 gallon
- MEDIUM NATIVE / ORNAMENTAL SHRUBS**
Rhododendron, Pieris, Viburnum, Berberis, Grasses, Laurel, Rockrose, minimum 2 gallon
- SMALL NATIVE / ORNAMENTAL SHRUB**
Grasses, Heather, Azalea, Lavender, Lily, minimum 1 gallon
- GROUND COVER**
Kinnikinnick, Rubus, Vinca, Wild Strawberry, Salal, minimum 1 gallon

MAIN STREET DESIGN
 LANDSCAPE ARCHITECTURE
 9402 Fidal Court
 Burien, WA 98148 (206) 842-7886
 LICENSE # 063

CONCEPTUAL LANDSCAPE PLAN
 SCALE: 1"=60'-0"
 LANCE MUELLER & ASSOCIATES
 ARCHITECTS • AIA
 130 LAKESIDE • SEATTLE, WA 98122 • 206.325.2553
 sheet 1-1

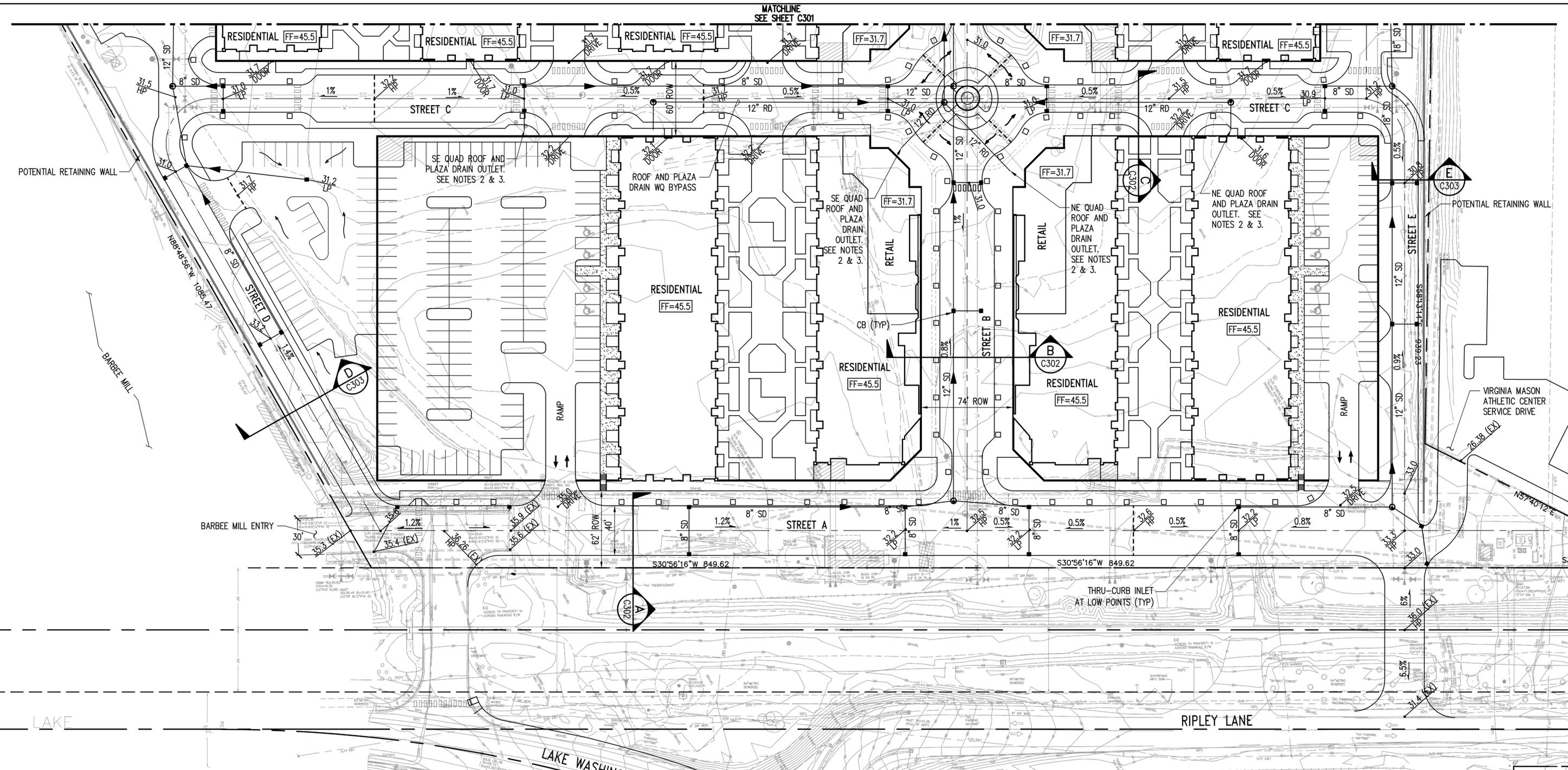
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		5	PREFERRED ALTERNATIVE REVISED	6-7-12
		4	PREFERRED ALTERNATIVE	5-2-12
		3	DRAFT 4/16/12	4-16-12
		2	DRAFT 4/16/12	4-16-12
		1	ALTERNATIVE #6	3-14-12

QUENDALL TERMINALS
 RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP

QUENDALL TERMINALS - PREFERRED ALTERNATIVE

RENTON, WASHINGTON
 CENTURY PACIFIC, LLLP
 EXHIBIT 11

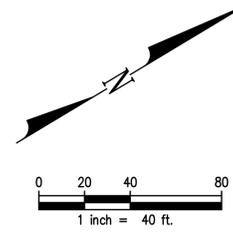
11-16-09
 N:/DWCS/09120/QUENDALL/P1-0



NOTES:

- ENVIRONMENTAL REMEDIATION AND MITIGATION OF THE PROPERTY WILL BE CONDUCTED PRIOR TO DEVELOPMENT. THE ENVIRONMENTAL PROTECTION AGENCY (EPA) IS THE LEAD AGENCY FOR ALL SITE REMEDIATION AND MITIGATION ACTIONS WHICH ARE TO BE PERFORMED AT THE QUENDALL TERMINALS SITE UNDER SUPERFUND.
- ADDITIONAL BUILDING ROOF DRAIN CONNECTIONS AND PIPING MAY BE REQUIRED.
- FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.

EXHIBIT 12



NOT FOR CONSTRUCTION

Z:\1500001-1500050 (Quendall Terminals) CADD\Design\07-C300-SD.dwg

Jan 12, 2016 - 8:43am

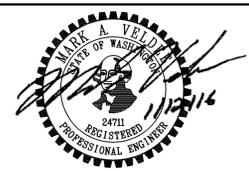
NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

DRAWN BY TAD	DESIGNED BY WTJ
CHECKED BY WTJ	APPROVED BY MAV
DATE NOV 16, 2009	
JOB No. :1500050	

CALL TWO BUSINESS DAYS BEFORE YOU DIG 1-800-424-5555

SCALE:
AS NOTED

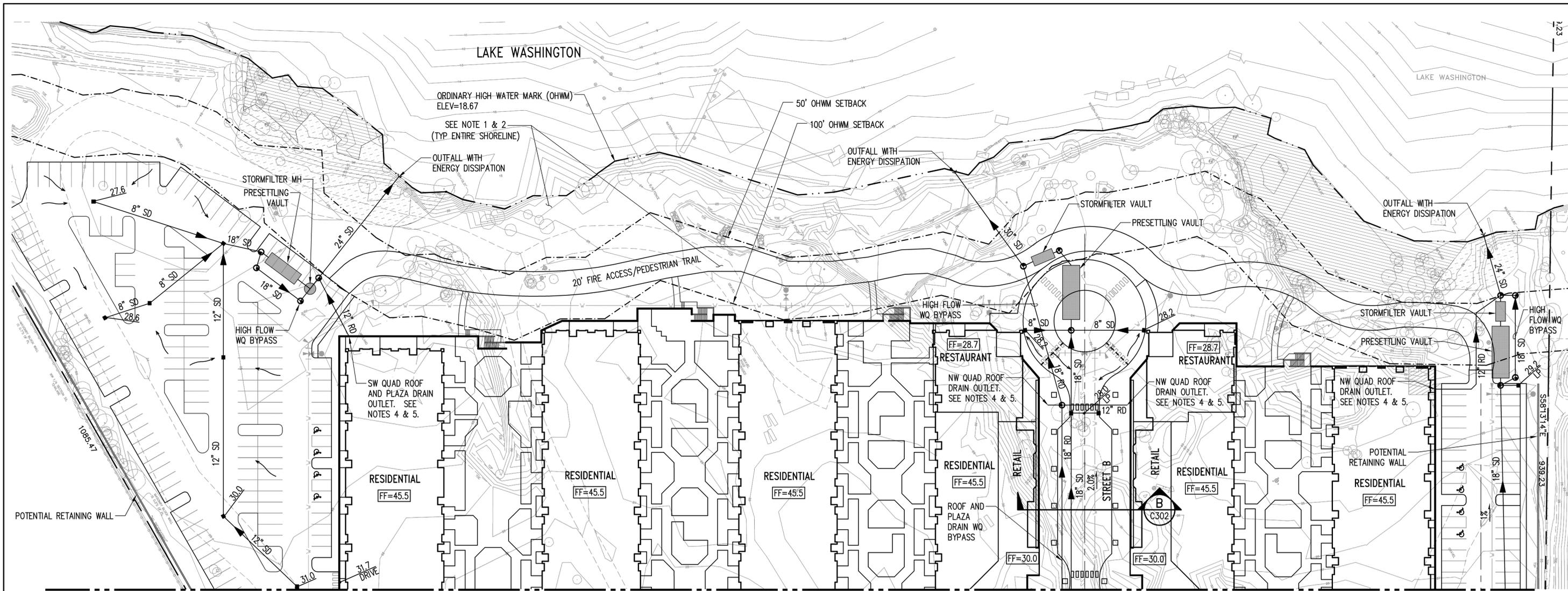
kpff
1601 5th Avenue, Suite 1600
Seattle, WA 98101
206.622.5822
www.kpff.com



QUENDALL TERMINALS
4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
CONCEPTUAL STORM DRAINAGE AND GRADING PLAN

SHEET
C300



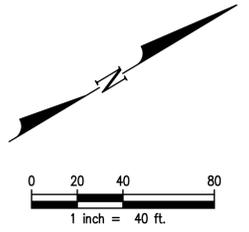
SEE SHEET C300
MACHLINE

LEGEND:

- EXISTING WETLAND (SEE NOTE 3)
- CONCEPTUAL WETLAND CREATION/RESTORATION (SEE NOTE 3)

NOTES:

1. ENVIRONMENTAL REMEDIATION AND MITIGATION OF THE PROPERTY WILL BE CONDUCTED PRIOR TO DEVELOPMENT. THE ENVIRONMENTAL PROTECTION AGENCY (EPA) IS THE LEAD AGENCY FOR ALL SITE REMEDIATION AND MITIGATION ACTIONS WHICH ARE TO BE PERFORMED AT THE QUENDALL TERMINALS SITE UNDER SUPERFUND.
2. THE PROJECT SITE INCLUDES APPROXIMATELY 1,583 FEET OF SHORELINE ALONG LAKE WASHINGTON. A 100-FOOT WIDTH RIPARIAN SETBACK (MEASURED FROM THE ORDINARY HIGH WATER MARK) AND PARALLELS ALONG THE SHORELINE. A SHORELINE RESTORATION PLAN IS BEING DESIGNED AND APPROVED UNDER EPA DIRECTION.
3. EXISTING WETLANDS AND CONCEPTUAL WETLAND CREATION/RESTORATION AREAS SHOWN HAVE NOT RECEIVED FINAL EPA DESIGN APPROVAL.
4. ADDITIONAL BUILDING ROOF DRAIN CONNECTIONS AND PIPING MAY BE REQUIRED.
5. FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.



NOT FOR CONSTRUCTION

DRAWN BY TAD	DESIGNED BY WTJ
CHECKED BY WTJ	APPROVED BY MAV
DATE NOV 16, 2009	
JOB No. :1500050	

**CALL TWO BUSINESS
DAYS BEFORE YOU DIG
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Seattle, WA 98101
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QUENDALL TERMINALS
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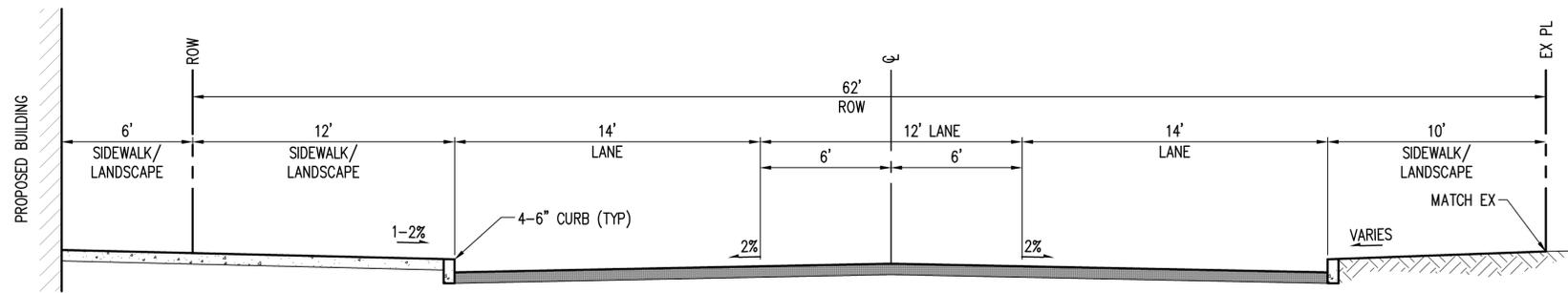
**LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
CONCEPTUAL STORM DRAINAGE AND GRADING PLAN**

SHEET

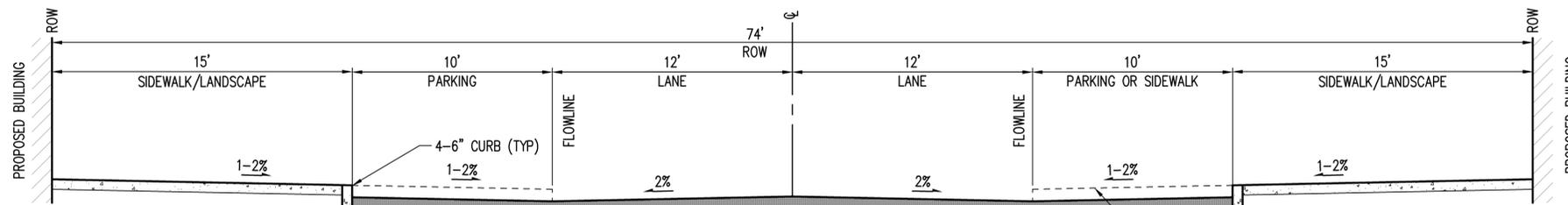
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NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

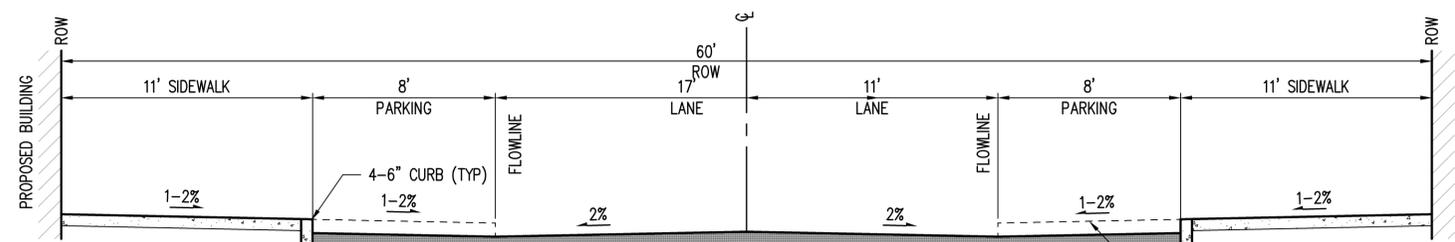
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 Jan 12, 2016 - 8:43am
 teresad



STREET A - TYPICAL ROADWAY SECTION (A)
SCALE: 1" = 4'



STREET B - TYPICAL ROADWAY SECTION (B)
SCALE: 1" = 4'



STREET C - TYPICAL ROADWAY SECTION (C)
SCALE: 1" = 4'

EXHIBIT 13

NOT FOR CONSTRUCTION

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teresad

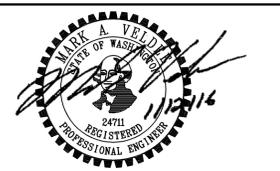
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NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

DRAWN BY TAD	DESIGNED BY WTJ
CHECKED BY WTJ	APPROVED BY MAV
DATE NOV 16, 2009	
JOB No.: 1500050	

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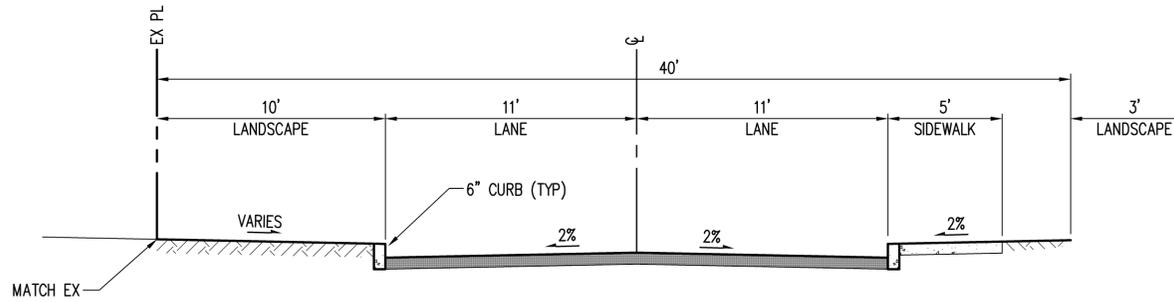
SCALE:
AS NOTED



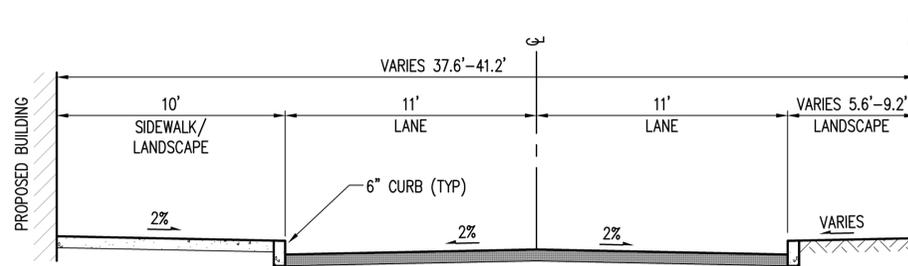
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4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
ROADWAY SECTIONS

SHEET
C302



STREET D – TYPICAL PRIVATE DRIVE SECTION D
SCALE: 1" = 4'



STREET E – TYPICAL PRIVATE DRIVE SECTION E
SCALE: 1" = 4'

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teresad

Jan 12, 2016 - 12:21 pm

NOT FOR CONSTRUCTION

NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

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SCALE:
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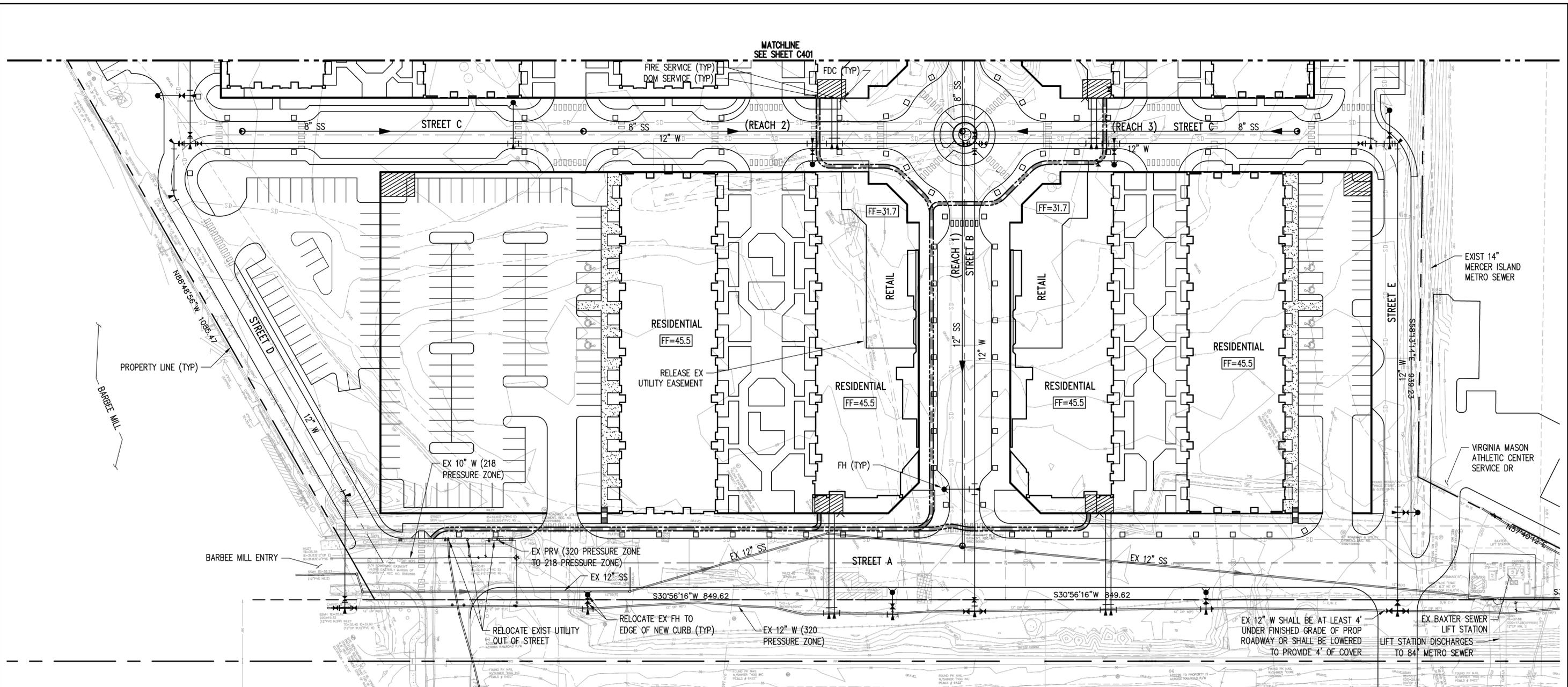
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4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
ROADWAY SECTIONS

SHEET
C303



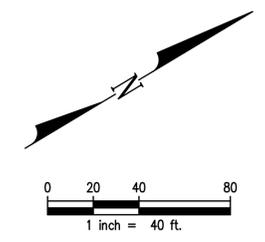
WATER NOTES:

- ALL NEW AND RELOCATED FIRE HYDRANTS SHALL BE PER COR STANDARD PLAN B102.
- ALL PROPOSED WATER MAINS ARE LOCATED WITHIN THE 320 PRESSURE ZONE.
- FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.

LEGEND:

- MECHANICAL SERVICE ROOM:**
- DOM PRV, METER AND RPBA
 - FIRE DDCVA
 - ACCESS DOOR FROM OUTSIDE
 - FDC ON BUILDING FACE
- FIRE HYDRANT
 - VALVE
 - FRANCHISE UTILITY DUCT BANK (COMM, GAS, POWER)
 - FF=32.5 FINISHED FLOOR ELEVATION

EXHIBIT 14



NOT FOR CONSTRUCTION

NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

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CHECKED BY WTJ	APPROVED BY MAV
DATE NOV 16, 2009	
JOB No. :1500050	

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SCALE:
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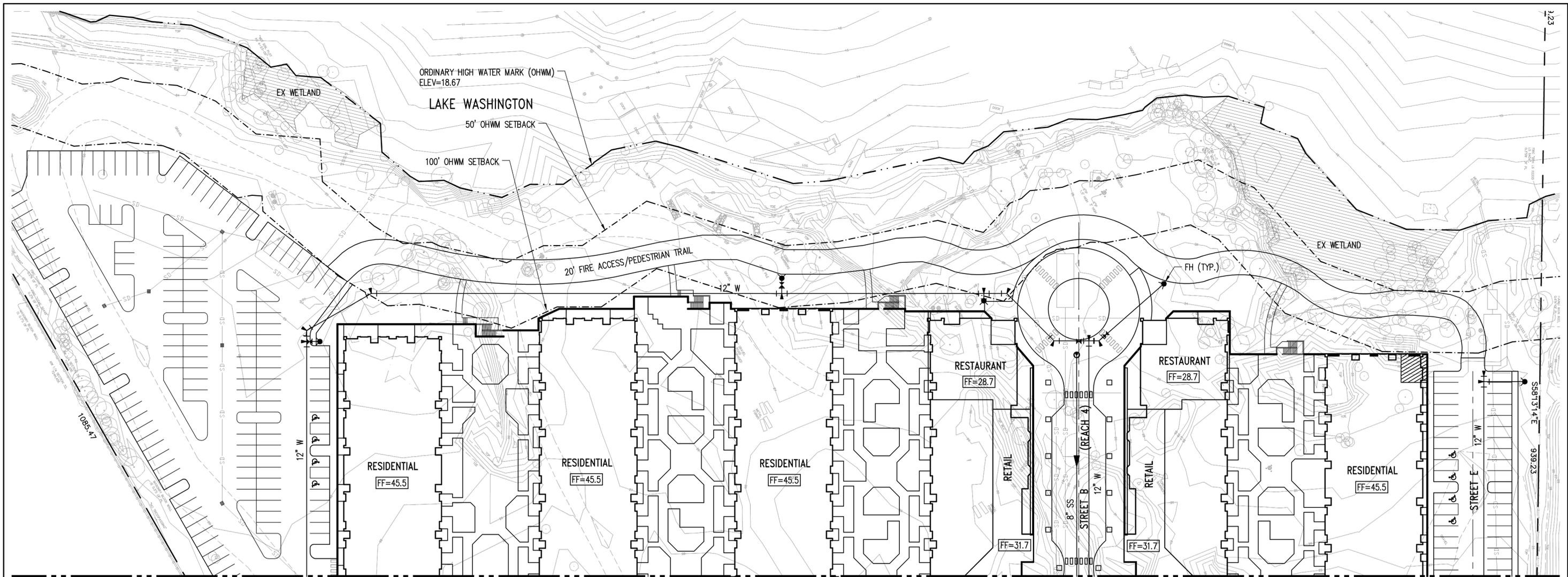


QUENDALL TERMINALS
4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
CONCEPTUAL UTILITY PLAN

SHEET
C400

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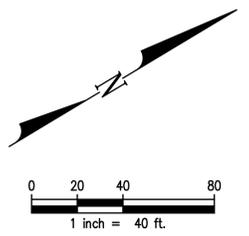
SEE SHEET C400
MACHLINE

WATER NOTES:

- ALL NEW AND RELOCATED FIRE HYDRANTS SHALL BE PER COR STANDARD PLAN B102.
- ALL PROPOSED WATER MAINS ARE LOCATED WITHIN THE 320 PRESSURE ZONE.
- FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.

LEGEND:

- MECHANICAL SERVICE ROOM:**
- DOM PRV, METER AND RPBA
 - FIRE DDCVA
 - ACCESS DOOR FROM OUTSIDE
 - FDC ON BUILDING FACE
- FIRE HYDRANT
 - ✕ VALVE
 - ▬▬▬ FRANCHISE UTILITY DUCT BANK (COMM, GAS, POWER)
 - FF=32.5 FINISHED FLOOR ELEVATION



NOT FOR CONSTRUCTION

NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

DRAWN BY TAD	DESIGNED BY WTJ
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SCALE:
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QUENDALL TERMINALS
4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
CONCEPTUAL UTILITY PLAN

SHEET
C401

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Jan 12, 2016 - 8:44am
teresad



January 12, 2016

Vanessa Dolbee, Current Planning Manager
City of Renton Department of Community
& Economic Development, Planning Division
1055 S Grady Way
Renton, WA 98057

RE: Quendall Terminal EIS Appeal

Dear Vanessa:

Per your request, EA has prepared the following summary of the opportunities that the City of Renton provided for involvement by the general public and U.S. EPA in the SEPA process for the Quendall Terminals project. The attached table summarizes the public's and EPA's involvement in the SEPA process, including the following information:

- Step in SEPA Process: e.g., DEIS, EIS Addendum, FEIS and EIS Appeal;
- Required: whether or not the step is required by SEPA;
- Provided: whether or not the step was provided for the Quendall Terminals EIS;
- Date(s): the dates on which the step was accomplished for the Quendall Terminals EIS;
- Duration: the duration of the step for the Quendall Terminals EIS, including whether it was extended beyond the duration required by SEPA; and
- Comments: comments on public/EPA involvement (e.g., the number of comment letters and emails received on the Quendall Terminals EIS Scoping, DEIS and EIS Addendum, and the way in which EPA's comments were incorporated into the EIS).

As shown by the attached table, the City went above and beyond the SEPA requirements to involve the public in the Quendall Terminals EIS process, including: extending the EIS Public Scoping period (from the required 21 days to 70 days); holding a Public Scoping meeting to provide additional opportunity for public comment (which is not required); extending the DEIS public scoping period (from the required 30 days to 60 days); holding a DEIS public hearing to provide additional opportunity for public comment (which is not required); and taking and responding to public comments on the EIS Addendum (which is not required).

The attached table also demonstrates that the City provided expanded opportunities for participation by EPA in the Quendall Terminals SEPA process and incorporated their input into the EIS, including: attending three meetings with EPA and the applicant to define the baseline assumptions for site cleanup/remediation that were used in the Draft EIS; and responding to

comments in two letters from EPA on the DEIS that ultimately resulted in new baseline cleanup/remediation assumptions that were used in the EIS Addendum (e.g., a new Preferred Alternative with an expanded setback from the Lake Washington shoreline was developed and analyzed in the Addendum).

Please let me know if you have any questions on this summary.

Sincerely,

A handwritten signature in black ink, appearing to read "Gretchen Brunner". The signature is fluid and cursive, with a large initial "G" and "B".

Gretchen Brunner, Senior Planner
EA Engineering, Science, and Technology, Inc. PBC

QUENDALL TERMINALS EIS PUBLIC & U.S. EPA INVOLVEMENT

PUBLIC INVOLVEMENT

<i>Step in SEPA Process</i>	<i>Required</i>	<i>Provided</i>	<i>Date(s)</i>	<i>Duration</i>	<i>Comments</i>
DEIS					
• EIS Public Scoping Period	Yes (21 days ¹)	Yes	2/19/10 – 4/30/10	70 days (extended)	5 letters/emails
• Public Scoping Meeting	No	Yes	4/27/10	1 day	4 commentators
• DEIS Public Comment Period	Yes (30 days ²)	Yes	12/10/10 – 2/09/11	60 days (extended)	75 letters/emails
• DEIS Public Hearing	No	Yes	1/04/11	1 day	8 commentators
EIS Addendum					
• EIS Addendum Public Comment Period	No	Yes	10/19/12 – 11/19/12	30 days	12 letters
FEIS					
• FEIS	Yes	Yes	8/31/15	N/A	- Responded to comments on DEIS and on EIS Addendum ³
EIS Appeal					
• EIS Public Appeal Period	Yes (20 days ⁴)	Yes	8/31/15 – 9/24/15	20 days	1 appellant

U.S. EPA INVOLVEMENT

<i>Step in SEPA Process</i>	<i>Required</i>	<i>Provided</i>	<i>Date(s)</i>	<i>Duration</i>	<i>Comments</i>
DEIS					
• Pre-EIS Mtgs. Re Baseline Assumptions	No	Yes	3/1/10, 4/22/10, 5/12/10	1 day each	- Baseline assumptions used in DEIS were based on input from EPA at Pre-EIS meetings
• Comment Letters on DEIS	No	Yes	1/13/11, 3/12/12	N/A	- Baseline assumptions used in EIS Addendum were modified based on comments on DEIS in EPA's 3/12/12 letter ⁵

¹ Per WAC 197-11-408(2)(i)

² Per WAC 197-11-455(6)

³ Taking and responding to comments on an EIS Addendum is not required by SEPA.

⁴ Per RMC 4-8-110E.1.b

⁵ In their 3/12/12 letter, U.S. EPA indicated that the environmental baseline (post-remediation conditions) assumptions represented in the DEIS are reasonable given the expected general outcome of the Record of Decision (ROD), with an increase of the minimum shoreline setback area to 100 feet from the lake (Lake Washington) edge. The Preferred Alternative analyzed in the EIS Addendum incorporated EPA's recommended shoreline setback.

Application Date: November 18, 2009
Name: Quendall Terminals

Site Address: 4350 Lake Washington Blvd N
 Renton, WA 98056

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Version 1 |

Engineering Review Comments

Contact: Rohini Nair | 425-430-7298 | rnair@rentonwa.gov

Recommendations: I have completed a preliminary review for the above referenced master site plan for the mixed use development which includes 692 residential units, 20,025 square feet of retail, and 9,000 square feet of restaurant. The following comments are based on the application submittal made to the City of Renton by the applicant.

General utility comments

1. All buried utilities, public roads, and infrastructure serving the site development shall be placed in clean fill material (with the utilities in a trench with sufficient width and depth of 3 to 4 feet below the invert and adjacent to the utility), along with an acceptable barrier to prevent recontamination of the clean fill material, in order to protect the utility from contamination and to allow future maintenance of the road or utility lines. (Mitigation Measure C29). This mitigation measure is applicable for both public and private utility lines.
2. The required horizontal and vertical separations as per City of Renton standards should be provided between the utility lines.
3. If the required minimum separation between utility lines need wider pavement width, then the street width should be changed accordingly.
4. Any existing utilities under the proposed buildings will be required to be abandoned and removed, and the easements will be required to be relinquished or amended.
5. All mitigation measures of the Quendall Terminals Mitigation Document shall be applicable on the project and should be provided by the project.
6. An agreement with King county for access and frontage improvements over King County owned railroad right of way should be provided to the City prior to site plan review application and construction permit application.

Water

The water utility main lines for this project will be public water lines. Minimum 15 feet wide easement should be provide to the City of Renton for the public water main located in private streets.

There is an existing 10 inch diameter water main on the King County parcel fronting the site and an 8 inch water main extending into the Quendall Terminals site.

1. The conceptual utility civil plans submitted should be revised to include the following:
 - Relocate about 870 feet of existing 12 inch water main along the property frontage to be within the new access road referred to as Street A. The existing water line cannot be accessible for repair and maintenance due to the location of the new road. Please see the attached water sketch.
 - Relocate the new 12 inch water main on the west side of the project to be within the paved 20 foot fire access road. The water main must be located at least 10 feet away from the building foundation and outside of the shoreline riparian area. To comply with these conditions, the buildings will need to be moved back further to the east to allow for the construction of the water main with the paved fire access road.
 - Complete the water main loop within the fire access road along the west side of the project from Street B to Street E.
 - Minimum 15 feet wide easement is required for water main.
2. All water mains and related appurtenances installed within the site shall be in accordance with the requirements of the EPA's approved plan for installation, operations, maintenance and monitoring plan of utilities.
3. Water mains shall be placed in clean fill materials, in a trench with sufficient width and depth of 3 to 4 feet below the invert of the water line, along with an acceptable barrier to prevent recontamination of the clean fill material, in order to protect the water mains from contamination and to allow future maintenance of the water mains by the City.
4. A utility easement and maintenance agreement with the city of Renton will be required for the maintenance and future repair of the water lines within the site. The property owners will be responsible for all costs related to the excavation, removal, and disposal of

PLAN - Planning Review - Land Use

Version 1 |

Engineering Review Comments**Contact:** Rohini Nair | 425-430-7298 | rnair@rentonwa.gov

materials and for final restoration associated with the City's operation, maintenance and repair of the water lines within the site.

5. Civil plans for the water main improvements that are submitted with the utility construction permit should be prepared by a professional engineer registered in the State of Washington. Please refer to City of Renton General Design and Construction Standards for Water Main Extensions as shown in Appendix J of the City's 2012 Water System Plan.

6. Payment of system development charge fee and permit fee will be required at the time of issuance of the utility construction permit. Fees that are current at the time of issuance of the utility construction permit will be applicable on the project.

Sanitary Sewer

The sewer utility main lines for this project will be public sewer lines. Minimum 15 feet wide easement should be provide to the City of Renton for the public sewer main located in private streets.

There is a 12 inch sewer main extending near the east property line of the Quendall Terminals parcel.

1. The sewer report mentions that the sewer system was designed to convey the peak flows by gravity to the project discharge location at a new manhole installed on an existing 12" diameter City of Renton sewer pipe.

2. Along with the utility construction permit plans, the developer is required to submit a revised sewer report that will reevaluate the existing Baxter lift station. The lift station capacity will need to be increased to serve the needs of the Quendall Terminals project. The sewer report submitted with the land use application showed an allowance of 1,100 gallons /acre/day for infiltration and inflow. The allowance number should be increased to 1,500 gallons/ acre/day.

3. Sewer manhole should be located outside of the landscaped center island on Street B.

4. Any use in the buildings (kitchen, restaurant, etc.) involving the handling of grease requires installation of a properly sized grease interceptor.

5. If the project proposes an indoor pool; the pool will need to be connected to the sanitary sewer system.

6. Storm drainage system within the indoor parking area shall be connected to an oil water separator and directed to the sewer system.

7. All buildings should be served by individual side sewers at a minimum.

8. Payment of system development charge fee and permit fee will be required at the time of issuance of the utility construction permit. Fees that are current at the time of issuance of the utility construction permit will be applicable on the project.

9. The Baxter lift station sewer Special Assessment District (SAD) fee will be applicable on the project. The base rate of this SAD fee is \$166,421 with an interest of 5.3%. The rate as March 22, 2016 is \$225,408.35 and will increase daily. This SDA fee rate will max out in July 2019. The rate that is current at the time of issuance of the utility construction permit will be applicable on the project. The payment will be due at the time of issuance of the utility construction permit.

Storm water

There is an existing 12 inch diameter stormwater line on North 42nd Place that ends near the west property line of the Quendall Terminals parcel.

Since the internal streets of the development are private, the storm water system for the development will be private. A stormwater covenant for allowing the City access to inspect the stormwater facility and assigning maintenance responsibility of the BMPs to the property owners will need to be recorded. The developer/ property owners/ HOA will be responsible for the maintenance of all stormwater systems constructed by the project.

1. A drainage plan and drainage report (TIR) based on the City of Renton Amendments to the 2009 King County Surface Water Manual should be submitted with the utility construction permit . The site is located in the Flow control Duration standard forested site conditions. The applicant is proposing to use the direct discharge exemption for the project. Water quality treatment is proposed for the project and should follow the requirements of the City of Renton Amendments to the 2009 King County Surface Water Manual. Storm water flow control BMPs are to be provided. All recommendations of the geotechnical report and the mitigation measures included in the final mitigation document should be followed in the design and construction of the project.

2. City of Renton has the Western Washington Phase II Municipal Stormwater permit. Per the requirements of the Phase II permit, all projects that have been approved prior to January 1 2017 and have not started construction by January 1, 2022 shall follow the new Surface Water Drainage Manual. Therefore, if the project has not started construction by January 1, 2022, the requirements of the Stormwater Manual that is current at that time will be applicable on the project.

3. The stormwater requirements (1 to10) included in the memorandum dated September 14, 2009 from Ronald Straka, Surface Water utility Supervisor, included below along with the additional information (a) and (b) are applicable on this project. The memorandum is

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Version 1 |

Engineering Review Comments
Contact: Rohini Nair | 425-430-7298 | rnair@rentonwa.gov

also included as an attachment.

- a. Projects approved prior to January 1, 2017 and have not started construction by January 1 2022, shall be subject to the requirements of the new Stormwater Manual that will be current at that time.
 - b. Projects that comply with the exceptions included in Section 1.2.8 of the 2009 Surface Water Manual may provide basic water quality treatment instead of enhanced basic water quality treatment.
4. Payment of system development charge fee and permit fee will be required at the time of issuance of the utility construction permit. Fees that are current at the time of issuance of the utility construction permit will be applicable on the project.

Transportation

1. Frontage improvements, including sidewalks, shall be provided along the west side of Lake Washington Boulevard and Ripley Lane N (Seahawks Way) along the site. These sidewalks shall connect to sidewalks to the north and south, which connect to other pedestrian facilities in the area. (Mitigation Measure G3.)
2. A crosswalk including pedestrian crossing warning signs at and in advance of the crosswalk shall be provided across Lake Washington Boulevard in order to connect the proposed development to the May Creek Trail on the east side of the Boulevard. The crosswalk shall be controlled by Rectangular Rapid Flashing Beacons, if the City determines that such lighting is warranted. (Mitigation Measure G9.)
3. A traffic mitigation/impact fee shall be determined and paid for the proposed development at the time of building permit issuance and in accordance with the City of Renton Municipal Code to help offset the impacts of the project on the City's roadways. (Mitigation Measure H1.)
4. TDM measures shall be implemented to reduce the number of vehicle trips and thus provide some benefit to improving LOS and queuing impacts at study intersections. (Mitigation Measure H2.)
5. Infrastructure improvements within the site shall include full curbs, gutters, sidewalks, and landscape strips (where applicable) as well as frontage improvements (curb, gutter, sidewalk, landscape strips, bike lanes, pavement width, and utilities) along the west side of Lake Washington Boulevard and Ripley Lane N in front of the project site. Provisions for safe pedestrian circulation shall encourage future transit usage to and from the site when planned public transit becomes available. (Mitigation Measure H3.)
6. If approved by EPA and any NRD settlement, a pedestrian trail shall be provided onsite through the minimum 100 foot shoreline setback area that shall be accessible to the public and shall connect to Lake Washington Boulevard through the internal site sidewalk system. If EPA's ROD or any NRD settlement prohibits the trail, the trail shall be relocated to the west side of the westernmost buildings onsite, and could be combined with the fire access road; this trail shall connect to Lake Washington Boulevard through the internal site sidewalk system. (Mitigation Measure H4.)
7. To mitigate traffic impacts to the Lake Washington Boulevard corridor south of the development, the applicant shall install traffic calming treatments on Lake Washington Boulevard south of N 41st Street to encourage primary trips generated by the project to utilize the I 405 corridor. Although the City of Renton has no adopted residential traffic management program, arterial calming measures could include treatments that create either horizontal or vertical deflection for drivers. Such treatments could include, but not limited to chicanes, serpentine raised curb sections, raised median treatments, speed tables, and/or speed humps. Final design of traffic calming elements shall be approved by the City. (Mitigation Measure H5.)
8. The parking supply under the Preferred Alternative shall meet the minimum off street parking requirements of the City of Renton. (Mitigation Measure H6.)
9. Shared parking agreements between on site uses and implementation of TDM measures for proposed residential uses shall be implemented to reduce parking demand during peak periods, thereby reducing the necessary parking supply. (Mitigation Measure H7.)
10. A fire access road shall be provided to the west of the westernmost buildings onsite. The road shall be a minimum of 20 feet wide, and shall be constructed with crushed rock or grass crete to support the weight of fire apparatus, and shall be available for emergency vehicle access. If located in the minimum 100 foot shoreline setback area, and approved by the EPA ROD and any NRD settlement, the road shall also serve as a pedestrian trail. If EPA's ROD or any NRD settlement prohibit the fire access road within the minimum 100 foot shoreline setback area, the road shall be relocated to the west side of the westernmost buildings onsite, and could be combined with the trail. (Mitigation Measure H8.)
11. In order to promote a multimodal transportation network, redevelopment on the Quendall Terminals site shall include site amenities (i.e., planting strips, street lighting, etc.) and access to future transit zones on Lake Washington Boulevard and at the I 405/NE 44th Street interchange to encourage and accommodate public transportation access in the future (future potential public transportation in the vicinity could include Bus Rapid Transit on I 405 planned by Sound Transit and Washington State Department of Transportation (WSDOT) with a flyer stop at the I 405/NE 44th Street interchange). (Mitigation Measure H9.)
12. Staff recommends that a paved bicycle lane shall be provided along the east and west sides of Ripley Lane (Seahawks Way)/Lake Washington Boulevard from the end of the current bike trail along Ripley Lane to the intersection of Ripley Lane (Seahawks Way)/Lake

PLAN - Planning Review - Land Use

Version 1 |

Engineering Review Comments**Contact:** Rohini Nair | 425-430-7298 | rnair@rentonwa.gov

Washington Boulevard (Mitigation Measure H10.)

13. The developer should coordinate with WSDOT, King County, and the City of Renton to finalize the required lane, signal, and frontage improvements on Lake Washington Blvd, Ripley Lane (Seahawks Way), and Barbee Mill access. This coordination and finalization of the street improvements and ROW requirements should take place before the site plan and the building/utility permit application is submitted to the City of Renton. All the street improvements included in the EIS, EIS Addendum, FEIS, and the mitigation document, to address the impacts of the project should be provided. Please see the figure titled 'Additional lanes required to be provided to mitigate project Impacts' for information regarding the additional turn lanes and additional through motor vehicular traffic lanes on Lake Washington Blvd. Street improvements should be constructed by the developer. The required ROW dedications should be provided and or obtained by the developer.

14. Private access at the Barbee Mill Access Frontage improvements including landscaped planter and sidewalk matching the existing improvements on the west side of the access is required to be provided on the east side of the access.

15. Private access at the Ripley Lane (Seahawks Way) 8 feet wide landscaped planter and 5 feet wide sidewalk is required to be provided on either side of the access.

16. For the scenario with I 405 improvements :

a. Lake Washington Blvd b/n Barbee Mill Access (N 43rd Street) and Ripley Lane N (Seahawks Way). The eastbound and westbound thru lanes planned by WSDOT shall be extended beyond and thru the Barbee Mill access intersection. This shall result in 2 thru lanes in each direction on Lake Washington Blvd from the I405 interchange past the Barbee Mill access (N 43rd Street).

b. Barbee Mill access (N 43rd Street)/ Lake Washington Blvd. Traffic signal shall be installed at the intersection. At the Barbee Mill Access (N 43rd Street) /Lake Washington Blvd intersection, the EB approach shall be widened to include a separate LEFT TURN only lane.

17. For the scenario without I 405 improvements:

a. Traffic Signals. Traffic signals shall be installed at the intersections of the I 405 northbound and southbound ramp intersections, as well as at the intersection of Barbee Mill Access (N 43rd Street)/Lake Washington Boulevard. The City will consider moving the location of this signal to the intersection at Ripley Lane/Lake Washington Boulevard as part of a future WSDOT improvement project to the NE 44th Street interchange. Relocating the traffic signal to Ripley Lane/Lake Washington Boulevard could reduce/eliminate potential longer range impacts of traffic queues on N 43rd Street between Lake Washington Boulevard and Road A, and with the existing rail crossing (should it be re activated for rail service or converted to a trail corridor). An engineering study will be completed at that time to support the determination of the location for the installation of the traffic signal at either N 43rd Street or Ripley Lane. (Mitigation Measure H 13.)

b. Intersection #1 I 405 Northbound Ramps/NE 44th Street. The southbound and northbound approaches shall be widened so that a separate left turn lane and shared thru right turn lane is provided on both legs of the intersection. The final configuration of the intersection with the additional widening improvements shall be coordinated with WSDOT. (Mitigation Measure H14.)

c. Mitigation measure H15 should be corrected to mention the widening on the eastbound approach on the Barbee Mill access instead of the previous typo that mentioned the westbound approach. [H15. Lake Washington Boulevard between Barbee Mill Access (N 43rd Street) and I 405 Southbound Ramps. Additional channelization improvements between the Barbee Mill access and the I 405 southbound ramps shall be constructed. Additional eastbound and westbound lanes shall be constructed to provide additional queue storage created by the traffic signals required at the southbound ramp and Barbee Mill Access (N 43rd Street) along Lake Washington Boulevard. At the Barbee Mill Access (N 43rd Street)/Lake Washington Boulevard intersection the eastbound approach on the Barbee Mill Access shall be widened to include a separate left turn only lane and the northbound approach on Lake Washington Boulevard shall be widened to include a separate left turn only lane. Ultimately, the City of Renton shall determine the best configuration for the improvements, given ongoing coordination with WSDOT on the adjacent interchange design, King County (owner of the vicinity rail right of way), and adjacent private development.]

d. Lake Washington Boulevard between Barbee Mill Access (N 43rd Street) and I 405 Southbound Ramps. Additional channelization improvements between the Barbee Mill access and the I 405 southbound ramps shall be constructed. Additional eastbound and westbound lanes shall be constructed to provide additional queue storage created by the traffic signals required at the southbound ramp and Barbee Mill Access (N 43rd Street) along Lake Washington Boulevard. At the Barbee Mill Access (N 43rd Street)/Lake Washington Boulevard intersection the westbound approach on the Barbee Mill Access shall be widened to include a separate left turn only lane and the northbound approach on Lake Washington Boulevard shall be widened to include a separate left turn only lane. Ultimately, the City of Renton shall determine the best configuration for the improvements, given ongoing coordination with WSDOT on the adjacent interchange design, King County (owner of the vicinity rail right of way), and adjacent private development. (Mitigation Measure H15.)

18. All the mitigation measures of the Quendall Terminals Mitigation Document shall be applicable on the project and should be provided by the developer prior to temporary occupancy certificate is given for the first building in the site.

19. All the internal streets of Quendall Terminals site shall be private streets.

20. The proposed cross section of the internal streets should be revised as per the attached drawings and as per the description

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Version 1 |

Engineering Review Comments

Contact: Rohini Nair | 425-430-7298 | rnair@rentonwa.gov

included below. If the street pavement width is not sufficient to accommodate the utility lines with the required separation as per the City of Renton standards, then the street widths will have to be increased accordingly.

a. Street A can have two cross sections depending on the use of the building on the side of the street. The cross section elements include

- i. Parking garage (residential use) near the street
 - 10 feet wide landscaping near the parking garage building
 - 12 feet wide sidewalk with tree grate (4'x8')
 - 0.5 feet wide curb
 - 6 feet wide on street parking on one side of the street
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 0.5 feet wide curb
 - 10 feet wide landscaping

- ii. No parking garage (retail use) near the street
 - 6 feet wide landscaping near the retail building
 - 12 feet wide sidewalk with tree grate (4'x8')
 - 0.5 feet wide curb
 - 6 feet wide on street parking on one side of the street
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 0.5 feet wide curb
 - 10 feet wide landscaping

An on site circulation study is required to be provided with the site plan to determine if the proposed 12 feet wide center turn lane is required. If the center turn lane is required, then the street width will have to be changed accordingly. The width of landscaping near the property line is also subject to change based on the site circulation study and/or the proposed use of the building adjacent to Street A.

b. Street B can have three cross sections depending on the use of the building on the side of the street. The cross section elements include

- i. No parking garage on either side of street B (retail use on both sides)
 - 15 feet wide sidewalk with tree grate (4'x8')
 - 0.5 feet wide curb
 - 8 feet wide on street parking on one side of the street
 - 24 feet wide paved travel way (2 12 feet wide lanes)
 - 8 feet wide on street parking on one side of the street
 - 0.5 feet wide curb
 - 15 feet wide sidewalk with tree grate (4'x8')

- ii. Parking garage (residential use) on one side of Street B
 - 10 feet wide landscaping near the parking garage building
 - 15 feet wide sidewalk with tree grate (4'x8')
 - 0.5 feet wide curb
 - 8 feet wide on street parking on one side of the street
 - 24 feet wide paved travel way (2 12 feet wide lanes)
 - 8 feet wide on street parking on one side of the street
 - 0.5 feet wide curb
 - 15 feet wide sidewalk with tree grate (4'x8')

- iii. Parking garage (residential use) on both sides of Street B
 - 10 feet wide landscaping near the parking garage building
 - 15 feet wide sidewalk with tree grate (4'x8')
 - 0.5 feet wide curb
 - 8 feet wide on street parking on one side of the street
 - 24 feet wide paved travel way (2 12 feet wide lanes)
 - 8 feet wide on street parking on one side of the street

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Engineering Review Comments

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- 0.5 feet wide curb
- 15 feet wide sidewalk with tree grate (4'x8')
- 10 feet wide landscaping near the parking garage building

- c. Street C can have three cross sections depending on the use of the building on either sides of the street. The cross section elements include:
 - i Parking garage (residential use) on both sides of Street C
 - 10 feet wide landscaping near the parking garage
 - 6 feet wide sidewalk
 - 0.5 feet wide curb
 - 6 feet wide on street parking lane on one side
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 6 feet wide on street parking lane on one side
 - 0.5 feet wide curb
 - 6 feet wide sidewalk
 - 10 feet wide landscaping near the parking garage
 - ii Parking garage (residential use) on one side of Street C
 - 10 feet wide landscaping near the garage
 - 6 feet wide sidewalk on the side near the garage
 - 0.5 feet wide curb
 - 6 feet wide on street parking lane on one side
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 6 feet wide on street parking lane on one side
 - 0.5 feet wide curb
 - 12 feet wide sidewalk on the side away from garage
 - 4 feet wide landscaping back of sidewalk on the side away from garage
 - iii No parking garage on any side of the street (retail use on both sides)
 - 4 feet wide landscaping near the building
 - 12 feet wide sidewalk
 - 0.5 feet wide curb
 - 6 feet wide on street parking lane on one side
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 6 feet wide on street parking lane on one side
 - 0.5 feet wide curb
 - 12 feet wide sidewalk
 - 4 feet wide landscaping back of sidewalk

- d. Street D cross section elements include:
 - 10 feet wide landscape setback
 - 0.5 feet wide curb
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 0.5 feet wide curb
 - 6 feet wide sidewalk
 - 5 feet wide landscaping between back of sidewalk and parking lot

- e. Street E cross section elements include:
 - 10 feet wide landscaping on the side near the parking garage
 - 6 feet wide sidewalk
 - 0.5 feet wide curb
 - 20 feet wide paved travel way (2 10 feet wide lanes)
 - 0.5 feet wide curb on the side near the property line

PLAN - Planning Review - Land Use

Version 1 |

Engineering Review Comments	Contact: Rohini Nair 425-430-7298 rnair@rentonwa.gov
<p><input type="checkbox"/> 5 feet landscape setback from property line</p> <p>If portions of Street E will have parking on both sides, then alternate street cross sections will be required for those portions of Street E.</p> <p>21. If the required minimum separation between utility lines need wider pavement width, then the street width should be changed accordingly.</p> <p>22. Street lighting is required to be provided on all streets. The street lighting can follow the City of Renton's residential street lighting requirements. Since the streets are private, the street lighting shall be privately owned and maintained by the developer/ property owner/ HOA.</p> <p>23. Parking garage entrances should be designed with consideration of sight distance.</p> <p>24. The proposed project has passed the City of Renton's traffic concurrency test. A traffic concurrency report has been provided for the project.</p> <p>25. An easement with King county for access, and an agreement with King County for construction of frontage improvements over King County owned railroad right of way should be provided to the City prior to site plan review application and construction permit application.</p> <p>General Comments</p> <p>1. All construction or service utility permits for drainage and street improvements will require separate plan submittals. All utility plans shall conform to the Renton Drafting Standards. Plans shall be prepared by a licensed Civil Engineer.</p> <p>2. When utility plans are complete, please submit four (4) copies of the drawings, two (2) copies of the drainage report, permit application, an itemized cost of construction estimate, and application fee at the counter on the sixth floor.</p> <p>3. All electrical, phone, and cable services and lines serving the proposed development must be underground. The construction of these franchise utilities must be inspected and approved by a City of Renton inspector prior to temporary certificate of occupancy.</p>	

Community Services Review Comments	Contact: Leslie Betlach 425-430-6619 LBetlach@rentonwa.gov
<p>Recommendations: ENVIRONMENTAL IMPACT COMMENTS (from Community Services)</p> <p>1. As per the Final EIS (Aug. 2015) and the Mitigation document (Aug. 2015), the revised plan sheets do not reflect the crosswalk across Lake Washington Blvd. as per Mitigation G 9 Condition.</p> <p>2. As per the final EIS (Aug. 2015) and the Mitigation Document (Aug. 2015), the revised plan sheets do not reflect the Trail connection within the 100' shoreline setback south to the Barbe Mill Development as per G 11 Mitigation Condition.</p>	

Planning Review Comments	Contact: Vanessa Dolbee 425-430-7314 vdolbee@rentonwa.gov
<p>Recommendations: 1. RMC section 4 4 030.C.2 limits haul hours between 8:30 am to 3:30 pm, Monday through Friday unless otherwise approved by the Development Services Division.</p> <p>2. Commercial, multi family, new single family and other nonresidential construction activities shall be restricted to the hours between seven o'clock (7:00) a.m. and eight o'clock (8:00) p.m., Monday through Friday. Work on Saturdays shall be restricted to the hours between nine o'clock (9:00) a.m. and eight o'clock (8:00) p.m. No work shall be permitted on Sundays.</p> <p>3. Within thirty (30) days of completion of grading work, the applicant shall hydroseed or plant an appropriate ground cover over any portion of the site that is graded or cleared of vegetation and where no further construction work will occur within ninety (90) days. Alternative measures such as mulch, sodding, or plastic covering as specified in the current King County Surface Water Management Design Manual as adopted by the City of Renton may be proposed between the dates of November 1st and March 31st of each year. The Development Services Division's approval of this work is required prior to final inspection and approval of the permit.</p> <p>4. A National Permit Discharge Elimination System (NPDES) permit is required when more than one acre is being cleared.</p> <p>5. This permit is shall comply with the Bald and Golden Eagle Protection Act. The permitted is responsible for adhering to the U.S. Fish and Wildlife Service National Bald Eagle Management Guidelines (2007) and /or your U.S. Fish and Wildlife Service permit.</p>	

Building Review - Planning Comments	Contact: Craig Burnell 425-430-7290 cburnell@rentonwa.gov
<p>Recommendations: follow recommendations of the soils report</p>	

PLAN - Planning Review - Land Use

Version 1 |

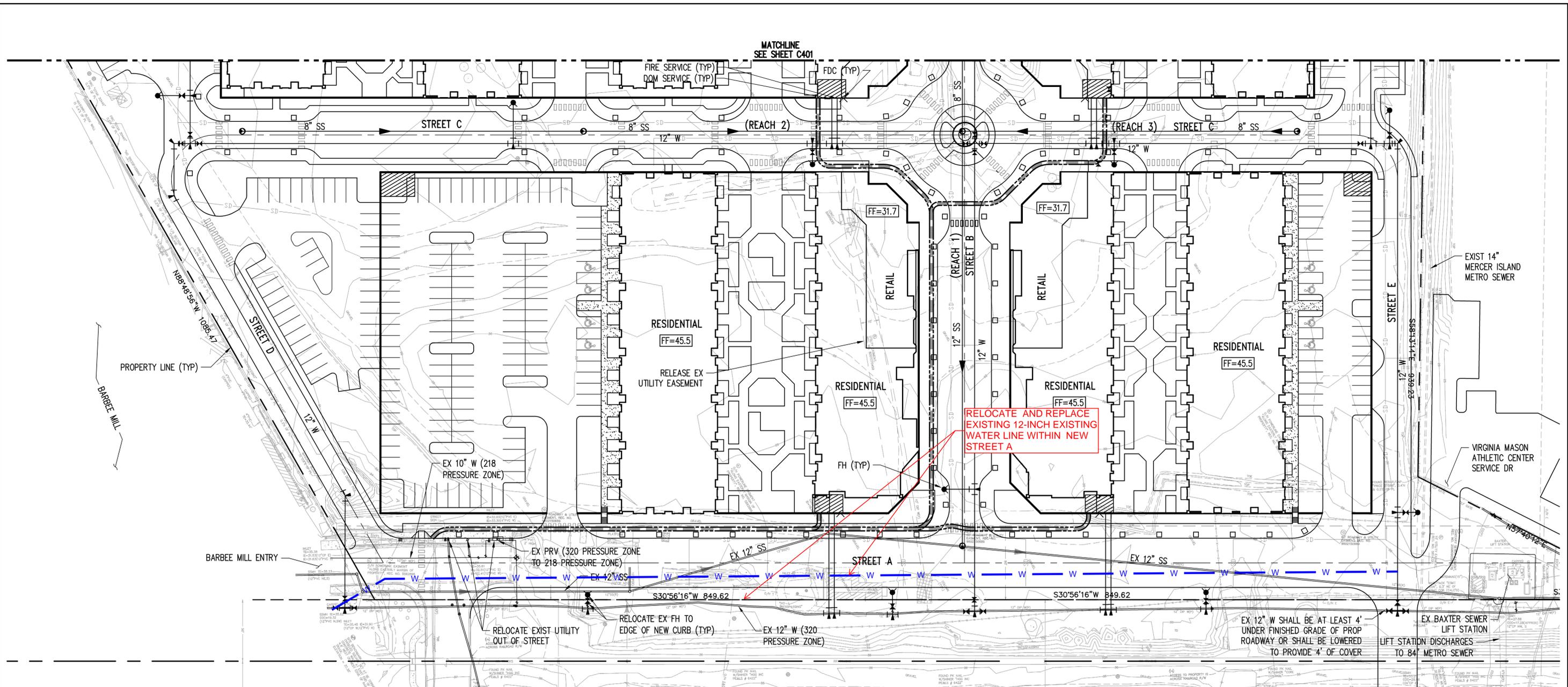
Technical Services Comments

Contact: Amanda Askren | 425-430-7369 | aaskren@rentonwa.gov

Recommendations: For the preliminary binding site plan review:

Updated title report not provided. Submitted title report for May 2009.

The dedication of land for street purposes on binding site plans requires approval by the City Council. Said dedication is achieved via a recorded City of Renton Dedication Deed document (form is provided by the city). If the dedication is to be recorded with the binding site plan, the dedication process needs to be timed in such a way that Council approval and all other matters pertaining to the dedication have been addressed and resolved, and said document is ready to record. The Deed of Dedication document includes both a legal description exhibit and a map exhibit. The legal description exhibit should be prepared, stamped, dated and signed by the applicant's surveyor. The surveyor should also prepare the map exhibit. The dedication process requires an updated title report, to be dated within the 45 days prior to Council action on said dedication. Talk to the Project Manager if there are questions or further information is needed.



WATER NOTES:

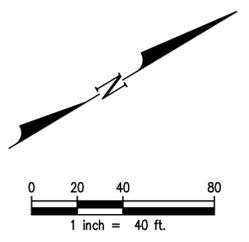
- ALL NEW AND RELOCATED FIRE HYDRANTS SHALL BE PER COR STANDARD PLAN B102.
- ALL PROPOSED WATER MAINS ARE LOCATED WITHIN THE 320 PRESSURE ZONE.
- FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.

LEGEND:

- MECHANICAL SERVICE ROOM:**
- DOM PRV, METER AND RPBA
 - FIRE DDCVA
 - ACCESS DOOR FROM OUTSIDE
 - FDC ON BUILDING FACE
- FIRE HYDRANT
- ✕ VALVE
- — — — — FRANCHISE UTILITY DUCT BANK (COMM, GAS, POWER)
- FF=32.5 FINISHED FLOOR ELEVATION

ALL WATER MAINS AND RELATED APPURTENANCES INSTALLED WITHIN THE SITE SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE EPA'S APPROVED PLAN FOR INSTALLATION, OPERATIONS, MAINTENANCE AND MONITORING PLAN OF UTILITIES. WATER MAINS SHALL BE PLACED IN CLEAN FILL MATERIALS, IN A TRENCH WITH SUFFICIENT WIDTH AND DEPTH OF 3 TO 4 FEET BELOW THE INVERT OF THE WATER LINE, ALONG WITH AN ACCEPTABLE BARRIER TO PREVENT RECONTAMINATION OF THE CLEAN FILL MATERIAL, IN ORDER TO PROTECT THE WATER MAINS FROM CONTAMINATION AND TO ALLOW FUTURE MAINTENANCE OF THE WATER MAINS BY THE CITY. AN UTILITY EASEMENT AND MAINTENANCE AGREEMENT WITH THE CITY OF RENTON WILL BE REQUIRED FOR THE MAINTENANCE AND FUTURE REPAIR OF THE WATER LINES WITHIN THE SITE. THE PROPERTY OWNERS WILL BE RESPONSIBLE FOR ALL COSTS RELATED TO THE EXCAVATION, REMOVAL AND DISPOSAL OF MATERIALS AND FOR FINAL RESTORATION ASSOCIATED WITH THE CITY'S OPERATION, MAINTENANCE AND REPAIR OF THE WATER LINES WITHIN THE SITE.

WATER PLANS REVIEW COMMENTS BY:
 ABDOUL GAPOUR - 425-430-7210
 CITY OF RENTON WATER UTILITY MANAGER
 DATE: 4/12/15



NOT FOR CONSTRUCTION

Z:\1500001-1500400\1500050 (Quendall Terminals) (CADD) Design\07-C400-UT.dwg
 teresad
 Jan 12, 2016 - 8:44am

NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

DRAWN BY TAD
 CHECKED BY WTJ
 DATE NOV 16, 2009
 J O B No. :1500050

DESIGNED BY WTJ
 APPROVED BY MAV
 DATE NOV 16, 2009

CALL TWO BUSINESS DAYS BEFORE YOU DIG
 1-800-424-5555

SCALE: AS NOTED

kpff
 1601 5th Avenue, Suite 1600
 Seattle, WA 98101
 206.622.5822
 www.kpff.com



QUENDALL TERMINALS
 4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
 CONCEPTUAL UTILITY PLAN

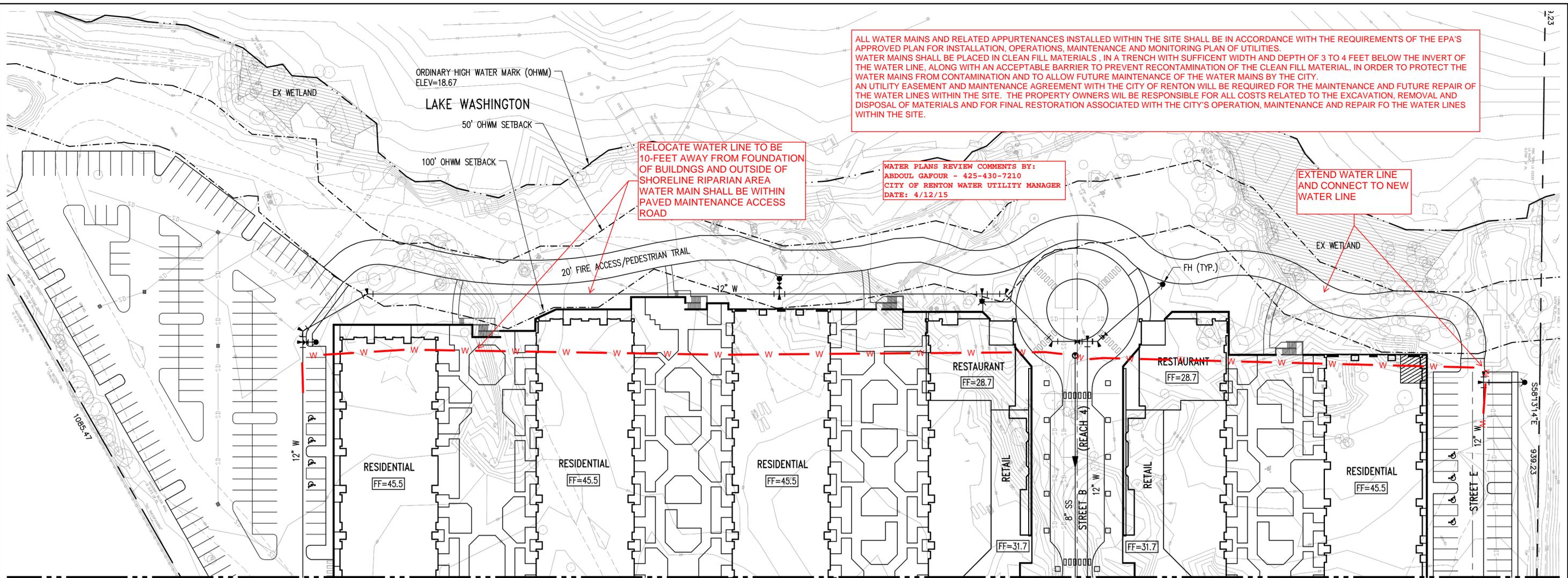
SHEET
C400

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RELOCATE WATER LINE TO BE 10-FEET AWAY FROM FOUNDATION OF BUILDINGS AND OUTSIDE OF SHORELINE RIPARIAN AREA WATER MAIN SHALL BE WITHIN PAVED MAINTENANCE ACCESS ROAD

WATER PLANS REVIEW COMMENTS BY:
 ABDOUL GAPOUR - 425-430-7210
 CITY OF RENTON WATER UTILITY MANAGER
 DATE: 4/12/15

EXTEND WATER LINE AND CONNECT TO NEW WATER LINE



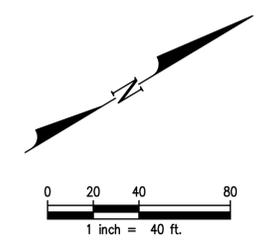
SEE SHEET C400 MACTHLINE

WATER NOTES:

1. ALL NEW AND RELOCATED FIRE HYDRANTS SHALL BE PER COR STANDARD PLAN B102.
2. ALL PROPOSED WATER MAINS ARE LOCATED WITHIN THE 320 PRESSURE ZONE.
3. FLEXIBLE UTILITY CONNECTIONS SHALL BE USED BETWEEN STRUCTURED BUILDINGS AND UNSTRUCTURED SITE AREAS TO MINIMIZE THE RISK OF DAMAGE TO UTILITIES DUE TO DIFFERENTIAL SETTLEMENT.

LEGEND:

- MECHANICAL SERVICE ROOM:**
- DOM PRV, METER AND RPBA
 - FIRE DDCVA
 - ACCESS DOOR FROM OUTSIDE
 - FDC ON BUILDING FACE
- FIRE HYDRANT
 - ✕ VALVE
 - ▬▬▬ FRANCHISE UTILITY DUCT BANK (COMM, GAS, POWER)
 - FF=32.5 FINISHED FLOOR ELEVATION



NOT FOR CONSTRUCTION

NO.	DATE	BY	CHD.	APPR.	REVISION
	12/30/15				REVISED PER MITIGATION REQUIREMENTS

DRAWN BY TAD	DESIGNED BY WTJ
CHECKED BY WTJ	APPROVED BY MAV
DATE NOV 16, 2009	
JOB No. :1500050	

CALL TWO BUSINESS DAYS BEFORE YOU DIG 1-800-424-5555

SCALE: AS NOTED



QUENDALL TERMINALS
 4350 LAKE WASHINGTON BOULEVARD, RENTON, WASHINGTON

LAND USE, SHORELINE & MASTER PLAN PERMIT APPLICATION
 CONCEPTUAL UTILITY PLAN

SHEET
C401

Z:\1500001-1500050 (Quendall Terminals) (CADD) Design\01-C400-UT.dwg
 teresad
 Jan 12, 2016 - 8:44am

M E M O R A N D U M

DEVELOPMENT SERVICES
CITY OF RENTON

SEP 18 2009

RECEIVED

DATE: September 14, 2009

TO: Arneta Henninger, Plan Reviewer

FROM:  Ronald J. Straka, Surface Water Utility Supervisor

SUBJECT: **QUENDALL TERMINALS STORMWATER REQUIREMENTS**

Please see my comments below regarding the drainage report and plans, dated August 27, 2009, for Quendall Terminals.

1. The project shall be required to comply with the 2009 King County Surface Water Design Manual (KCSWDM) standards as a condition of SEPA.
2. A complete TIR for the project site will be required with submittal of the civil engineering plans for the site that includes all information required by the 2009 King County Surface Water Design Manual. This includes information related to satisfying all core requirements and applicable special requirements in the 2009 KCSWDM.
3. The proposed projects must submit an offsite analysis report that assesses potential offsite drainage and water quality impacts associated with development of the project site and proposes appropriate mitigation of those impacts.
4. The report must include a KCRTS printout of all land use input values for pre- and post-developed impervious and pervious areas. The report should also include a basin summary table for the existing condition and developed condition land use. The basin summary table should identify the wetlands.
5. The wetland area needs to be included in the pre- and post-developed time series analysis.
6. Since the project will result in more than 5,000 square feet of new impervious surface, the project must comply with section 1.2.3 of the 2009 KCSWDM Flow Control. The direct discharge exemption may apply to the project if it meets all of the following criteria:
 - a. The flowpath from the project site discharge point to the edge of the 100-year floodplain of the major receiving water will be no longer than a quarter mile.
 - b. The conveyance system between the project site and the major receiving water will extend to the ordinary high water mark, and will be comprised

of manmade conveyance elements (pipes, ditches, etc.) and will be within public right-of-way or a public or private drainage easement

- c. The conveyance system will have adequate capacity per Core Requirement #4, Conveyance System, for the entire contributing drainage area, assuming build-out conditions to current zoning for the equivalent area portion and existing conditions for the remaining area
- d. The conveyance system will be adequately stabilized to prevent erosion, assuming the same basin conditions as assumed in Criteria (c) above.

NOTE: The major receiving waters do not include wetlands.

7. The engineer proposes to discharge runoff from the north and west portions of the site into the wetlands located at the north and west corners of the site. The engineer needs to conduct an analysis of the wetland to determine the existing hydrology, including the hydroperiod, and base the recharge on that analysis. Typically, the wetland report would include a recommendation from the biologist as to the proper recharge rate.
8. The proposed roadway improvements shall be taken into consideration when calculating the post development site condition and sizing the water quality and flow control facilities, if required.
9. The project will be required to provide enhanced water quality treatment per section 1.2.8.1. Application of the Enhanced Basic Water Quality menu may be waived for treatment of any runoff that is discharged entirely by pipe all the way to the ordinary high water mark to the major receiving waters, listed on pages 1-37 of the 2009 KCSWDM. Enhanced Basic Water Quality treatment is required for any discharges to the existing wetlands on site.
10. Does the project have an expected average daily traffic (ADT) count of 100 or more vehicles per square feet of gross building area, per section 1.2.8.1 of the 2009 KCSWDM? If yes, the project must provide oil control in addition to any other water quality facility required.

If you have any questions contact Hebé Bernardo, Surface Water Utility Engineer (x7264).

cc: Lys Hornsby, Utility Systems Director
Kayren Kittrick, Development Engineering Supervisor
Chip Vincent, Planning Director
Neil Watts, Development Services Director

1. Introduction

Quendall Terminals is a proposed mixed-use development in Renton, Washington. The development includes five stories of residential or office space above two levels of above-grade parking or retail and restaurant space. The development project anticipates entitlement of the following:

Table 1-1: Proposed Development

Use	Quantity/Area
Residential	800 Units
Office	245,000 Square Feet
Retail	21,600 Square Feet
Restaurant	9,000 Square Feet
Parking	2,215 Spaces

Note: All areas shown are gross building areas (GBA).

The project site is located west of Interstate 405 near the northern city limits of Renton. The site is bounded by the Seahawks Training Facility to the north, BNSF railroad tracks to the east, and the Barbee Mill residential community to the south. Ripley Lane is located east of the BNSF railroad tracks and Lake Washington Boulevard is located southeast of the project site. See Figure 1 in the Appendix for the site location.

This report is intended to support City of Renton entitlement processing for Master Site Plan Approval. The scope of this report is to address the sanitary sewer system for the proposed development. Design criteria will be outlined and a sewerage approach will be evaluated.

2. Predeveloped Site Conditions

The existing site is vacant and is the former location of a log sorting and storage yard. The main site is approximately 20.30 acres in size, and the parcel east of the main project site across Ripley Lane North is approximately 1.15 acres in size. An existing 12-inch sanitary sewer main runs from south to north along the east side of the site within a 60-foot roadway and utility easement. The invert elevation of the existing sewer pipe is generally 10 to 13 feet below the existing ground surface. The existing Baxter Lift Station serves Quendall Terminals as well as the Seahawks Training Facility to the north and Barbee Mill to the South. There are no other sewers located on the project site. An 84-inch Metro sewer main is located approximately 100 feet east of the site's east property line. See Figure 2 in the Appendix for existing site conditions.


 THERE IS ALSO A KING COUNTY FORCE MAIN THAT RUNS IN THE VICINITY OF THE NORTH PROPERTY LINE THAT IS PART OF THE MERGER ISLAND CONNECTION. CONTACT ERIC DAVISON @ 206.684.1207 TO GET DETAILED PLANS. (WOULD IMPACT WATER MAIN LOCATION FOR

3. Developed Site Conditions

The proposed site improvements include a mixed-use development consisting of residential, office, retail, and restaurant uses, as well as new public and private streets and parking. Sewer mains will be constructed within the proposed public streets. Sewage from the buildings will discharge to the new sewer mains via side sewers. The new sewer mains will discharge to the existing 12-inch sewer main at the east side of the project site at a new manhole constructed over the existing main. No improvements are planned for the 1.15-acre parcel east of Ripley Lane. See Figures 3 and 4 in the Appendix for proposed site plan and proposed sewer plan, respectively.

4. Basis of Design

An on-site sanitary sewer system will collect and convey flows from Quendall Terminals. Adjacent sites are already developed and served by separate sanitary sewer systems. This report has utilized programmed project areas and Department of Ecology (DOE) criteria to establish projected sewer flows without provisions for future growth or connections. See Figures 3 and 4 in the Appendix for proposed site plan and proposed sewer plan, respectively.

Gross building areas have been used for this report.

An allowance of 1,100 gallons/acre/day (gpad) has been made for infiltration and inflow since the proposed sanitary sewer system is expected to be below seasonal high groundwater elevations. The 1.15-acre parcel east of Ripley Lane has not been included in the infiltration calculation.

A peaking factor of 4.0 was included in the design flows. This factor should account for the daily and seasonal fluctuations in waste generation. This factor should also mitigate the impact of the varying flow generations for the different uses proposed with this project.

The sanitary sewer system was designed to convey the estimated peak flows by gravity to the project discharge location at a new manhole installed on an existing City of Renton sanitary sewer pipe. The sewer capacities were established using Manning's Equation, with an "n" factor of 0.013. Sewer lines have been designed using the minimum slope requirements of the Washington State DOE. The pipe slopes used in the final design and future construction documents may be greater than the minimum slope to accommodate potential settlement, depending on the recommendation of the geotechnical engineer.

WE CAN LOOK AT LOWERING THIS P.F. SINCE IT
WILL BE A FIXED DEVELOPMENT TO 2.0.

KPII

EXISTING BAXTER LIFT STATION

The Baxter Lift Station is an existing sewer lift station located at the northeast corner of the project site within a sanitary sewer easement. The lift station was designed in 2006 and was constructed in 2009. The lift station was designed for an overall peak flow of 594 gpm for the Seahawks Training Facility, Barbee Mill community, and the Quendall Terminals site. The lift station was designed and constructed with the following assumptions for future development of the Quendall Terminals site:

Table 5-3: Lift Station Design Assumptions for Quendall Terminals (per Figure 6)

Developable Acres	5	Tributary Area	5.0 Acres
Flow Rate	2,800 gpad		
Number of Units	.75	Tributary Area	3.0 Acres
Persons/Unit	2.4		
Flow Rate	100 gpad		
Average Sewerage Flow	22.2 gpm	Design I/I Rate	1,500 gpad
Peaking Factor	4		
Design Sewage Flow	88.9 gpm	Design I/I Flow	8.3 gpm
Total Design Flow	97.2 gpm	Total Design Flow	Q peak hourly

The sewer lift station was designed for a flow of 97.2 gpm from the Quendall project site. The anticipated flow from the Quendall project site is 614 gpm. The sewer lift station capacity will need to be increased by approximately 517 gpm to 1,111 gpm to accommodate development of the Quendall Terminals site. Per discussion with the City of Renton Public Works, the existing lift station has the ability to be modified to increase capacity by changing pump impellers and increasing the wet well capacity. See Figure 6 in the Appendix for Baxter Lift Station design details and Figure 7 for a record of discussion with the City.

However, given the total increase in flow, there is the potential need to ~~replace~~ ~~four~~ ~~replace~~ out pumps and add additional storage. ~~The increased flow~~ ~~bottom line~~ is a flow analysis needs to be performed as we may have additional capacity at the station now that we have actual flows at the station from the VMAC creater. The developer will be responsible for all improvements needed to accommodate additional flows.

- Other available options are installing larger pumps but Dave did not believe that would be necessary as the existing pumps were specifically chosen to allow impeller modifications as they anticipated the need for additional capacity in the future.

Fees:

- Quendall has recently been assessed a capacity charge of \$166k for their "fair share" of the Baxter Pump Station. This assessment was based on 111gpm of capacity. I asked Dave if future assessments would be required if the flows exceeded the 111gpm. Dave indicated there would be **NO** additional capacity charge assessments for the Quendall site only mechanical pump station upgrades to increase the pump station capacity to meet our proposed site demand.

*STANDARD SYSTEM
DEVELOPMENT CHARGES WILL APPLY BASED UPON
DOMESTIC METER SIZES.*

BUILDING USE AND DISCHARGE POINT PER TRIBUTARY AREA

Trib. Area ID	Resid. [UNITS]	Office [SF]	Retail [SF]	Rest. [SF]	Discharge To
NE Trib. Area	0	117500	4800	0	Reach 3
SE Trib. Area	175	107500	4500	0	Reach 2
SW Trib. Area 1	360	0	0	0	Reach 2
SW Trib. Area 2	90	10000	6300	4500	Reach 4
NW Trib. Area 1	100	0	0	0	Reach 3
NW Trib. Area 2	75	10000	6000	4500	Reach 4
Total	800	245000	21600	9000	Reach 1

BUILDING USE PER REACH

Reach ID	Resid. [UNITS]	Office [SF]	Retail [SF]	Rest. [SF]	Rest. [SEATS]
Reach 1	800	245000	21600	9000	396
Reach 2	535	107500	4500	0	0
Reach 3	100	117500	4800	0	0
Reach 4	165	20000	12300	9000	396

INFILTRATION/INFLOW

Site Size [AC]	20.3
Flow [GPD/AC]	1100
Flow [GPM]	16

1500 is CITY STANDARD

UNIT FLOW AND PEAK FACTOR PER BUILDING USE

Use	Unit Flow [GPD]	Peak Factor
Residential	175 [per unit]	4
Office	0.2 [per sq ft]	4
Retail	0.3 [per sq ft]	4
Restaurants	50 [per seat]	4
I/I ₄	1100 [per acre]	1

1500

NOTES

- Unit flows include normal infiltration
- Assumes 1.75 residents per unit
- Restaurant conversion: 1 seat = 22.7 square feet of restaurant
- Infiltration due to high groundwater

FLOW PER REACH

Reach ID	Resid. [GPM]	Office [GPM]	Retail [GPM]	Rest. [GPM]	I/I [GPM]	Total [GPM]
Reach 1	389	136	18	55	16	614
Reach 2	260	60	4	0	4	328
Reach 3	49	65	4	0	4	122
Reach 4	80	11	10	55	4	161

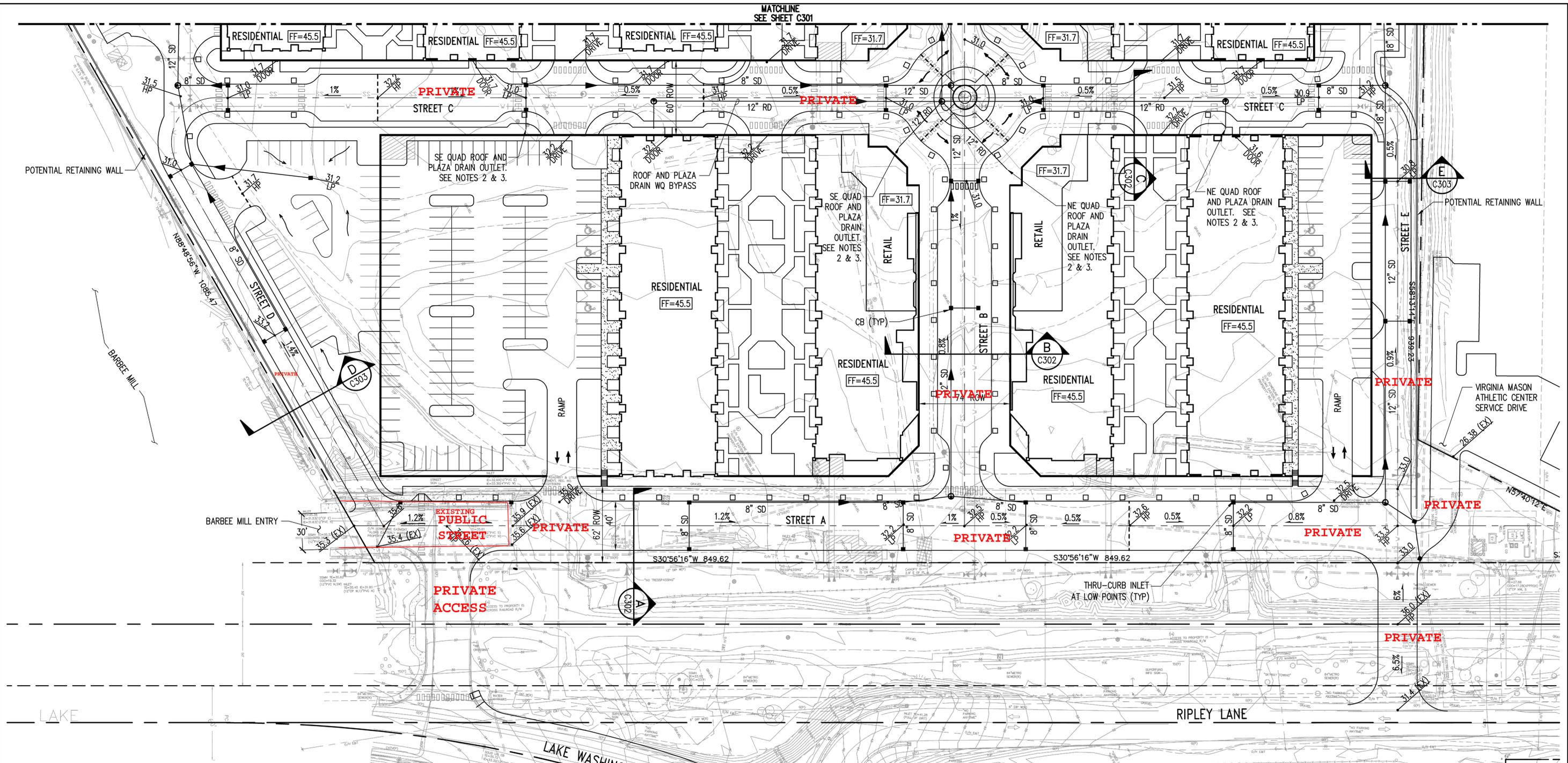
PIPE CALCULATIONS

Reach ID	Upstrm. MH	Downstrm. MH	Length [FT]	Inner Dia [IN]	Upstrm. IE [FT]	Downstrm. IE [FT]	Slope [FT/FT]	n	Q _{full} [CFS]	Q _{full} [GPM]	Q _{design} [GPM]	% Cap.	V _{full} [FPS]	Origin of Flow
REACH 1	SSMH #2	SSMH #1	335	12	19.23	18.49	0.0022	0.013	1.68	754	614	81%	2.14	REACH 2, REACH 3, REACH 4
REACH 2	SSMH #3S-2	SSMH #3S-1	278	8	21.88	20.77	0.0040	0.013	0.77	344	328	95%	2.19	SE Trib. Area, SW Trib. Area 1
REACH 3	SSMH #3S-1	SSMH #2	278	8	20.67	19.56	0.0040	0.013	0.77	344	328	95%	2.19	NE Trib. Area, NW Trib. Area 1
REACH 4	SSMH #3N	SSMH #2	340	8	20.92	19.56	0.0040	0.013	0.77	344	122	35%	2.20	SW Trib. Area 2, NW Trib. Area 2
REACH 4	SSMH #3W	SSMH #2	271	8	20.64	19.56	0.0040	0.013	0.76	343	161	47%	2.19	SW Trib. Area 2, NW Trib. Area 2

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- NOTES:**
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	12/30/15				REVISED PER MITIGATION REQUIREMENTS

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 APPROVED BY MAV
 DATE NOV 16, 2009
 J O B No. :1500050

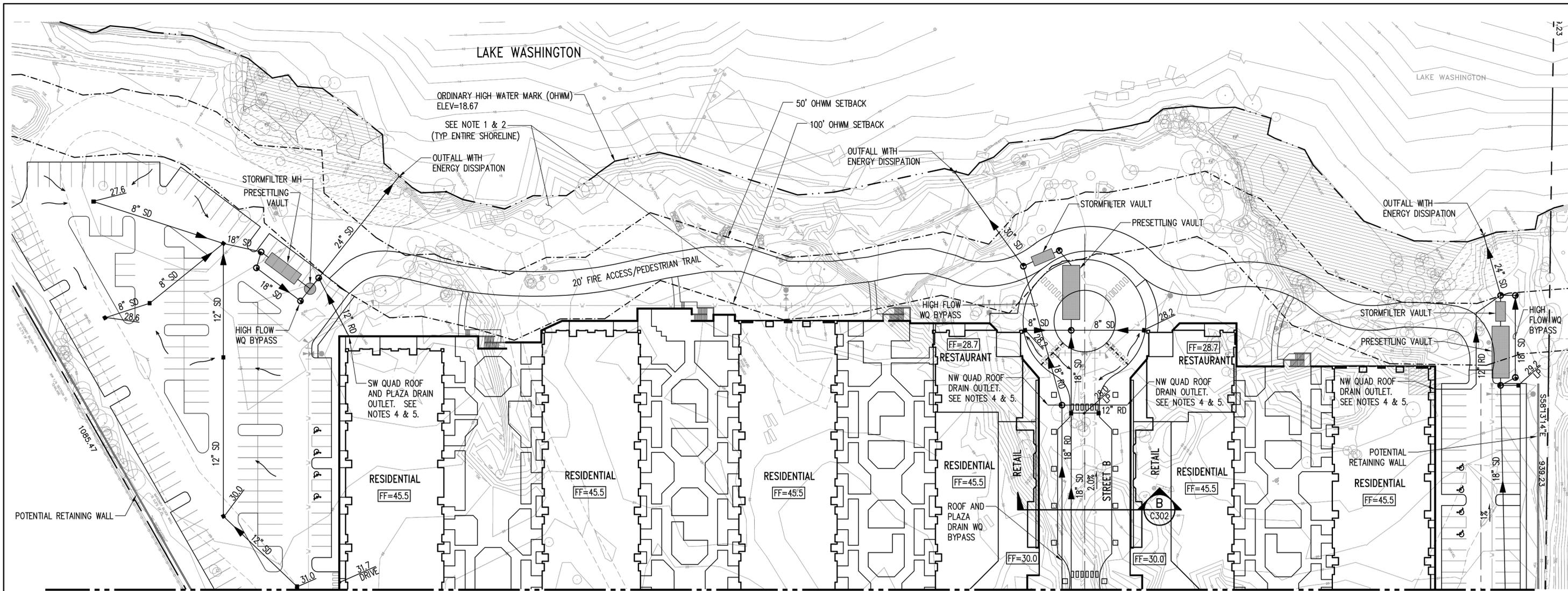
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CONCEPTUAL STORM DRAINAGE AND GRADING PLAN

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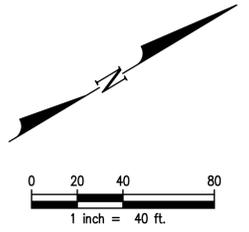
SEE SHEET C300
MACHLINE

LEGEND:

- EXISTING WETLAND (SEE NOTE 3)
- CONCEPTUAL WETLAND CREATION/RESTORATION (SEE NOTE 3)

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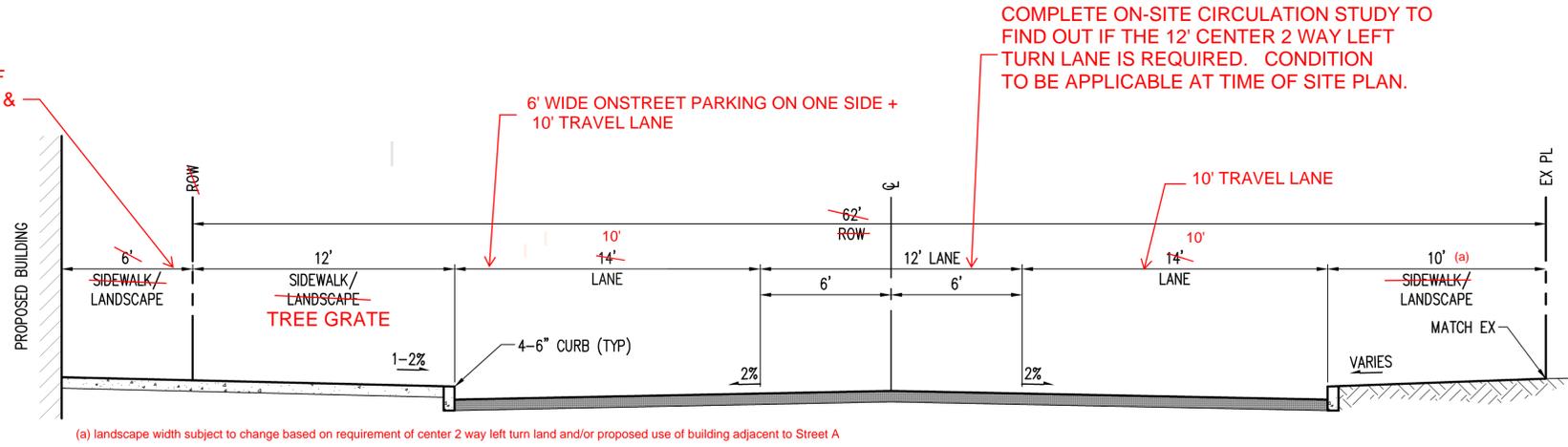
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10 FEET WIDE LANDSCAPE IS REQUIRED IF LOCATED IN FRONT OF PARKING GARAGE, & 6 FEET WIDE LANDSCAPE IS REQUIRED IF LOCATED IN FRONT OF RETAIL USE.

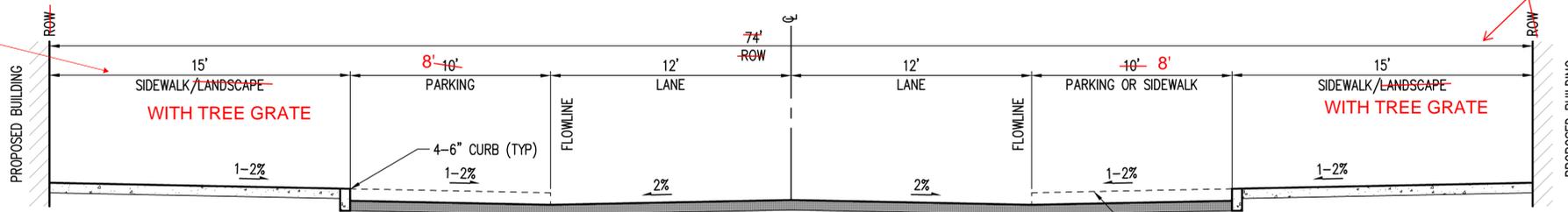


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THE CROSS SECTION ELEMENTS AND WIDTHS ON STREET A WILL VARY DEPENDING ON THE USE OF BUILDINGS ON THE SIDE OF THE STREET & IF THE CENTER TURN LANE IS REQUIRED.

STREET A - TYPICAL ROADWAY SECTION (A)
SCALE: 1" = 4'

ADDITIONAL 10 FEET WIDE LANDSCAPING IS REQUIRED BETWEEN THE SIDEWALK AND THE BUILDING IF THE BUILDING HAS A PARKING GARAGE ADJACENT TO THE STREET.

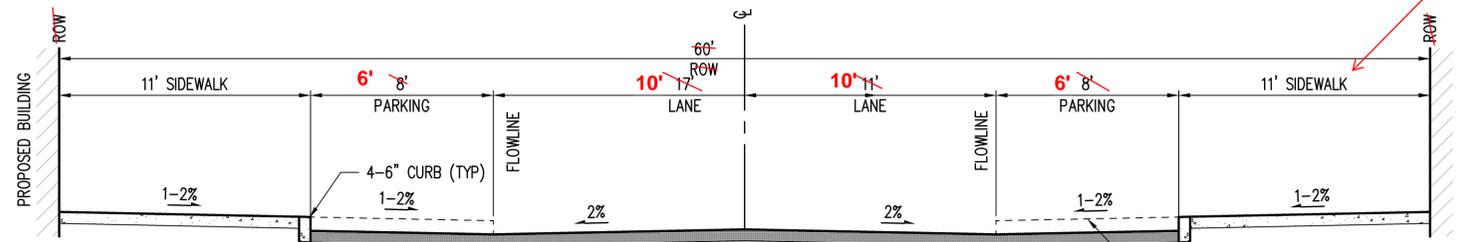


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STREET B - TYPICAL ROADWAY SECTION (B)
SCALE: 1" = 4'

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STREET C - TYPICAL ROADWAY SECTION (C)
SCALE: 1" = 4'

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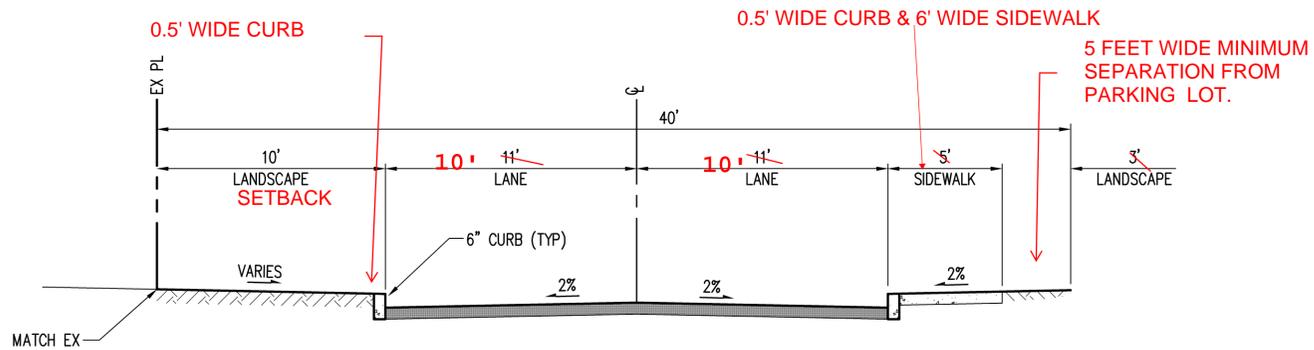
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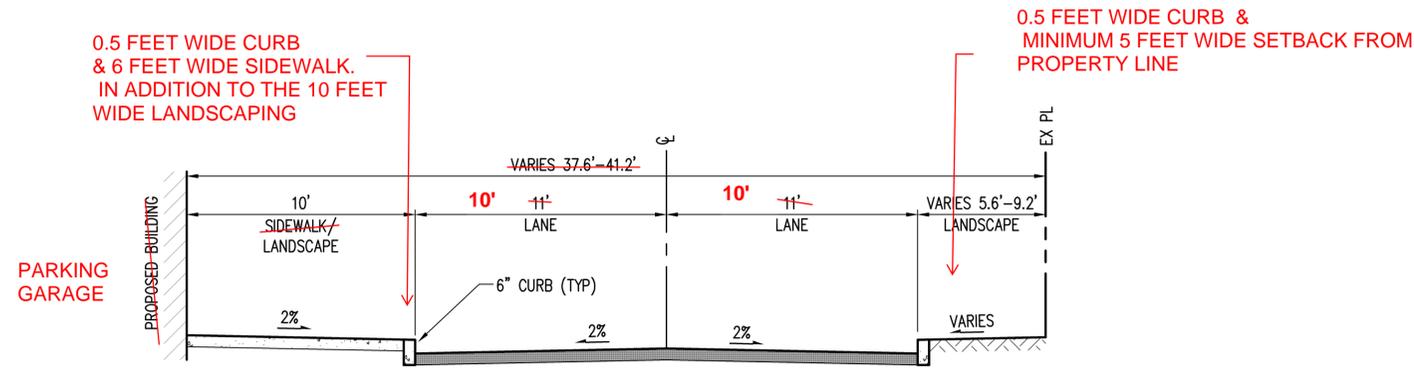
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ROADWAY SECTIONS

SHEET
C302



STREET D – TYPICAL PRIVATE DRIVE SECTION D
SCALE: 1" = 4'



STREET E – TYPICAL PRIVATE DRIVE SECTION E
SCALE: 1" = 4'

ALTERNATE SECTION SHOULD ALSO BE PROVIDED FOR THE PORTION OF STREET E WITH PARKING ON BOTH SIDES.

GENERAL NOTE: IF ADDITIONAL PAVED WIDTH IS REQUIRED TO PROVIDE THE MINIMUM SEPARATION BETWEEN UTILITY LINES, THE STREET WIDTH SHOULD BE INCREASED ACCORDINGLY.

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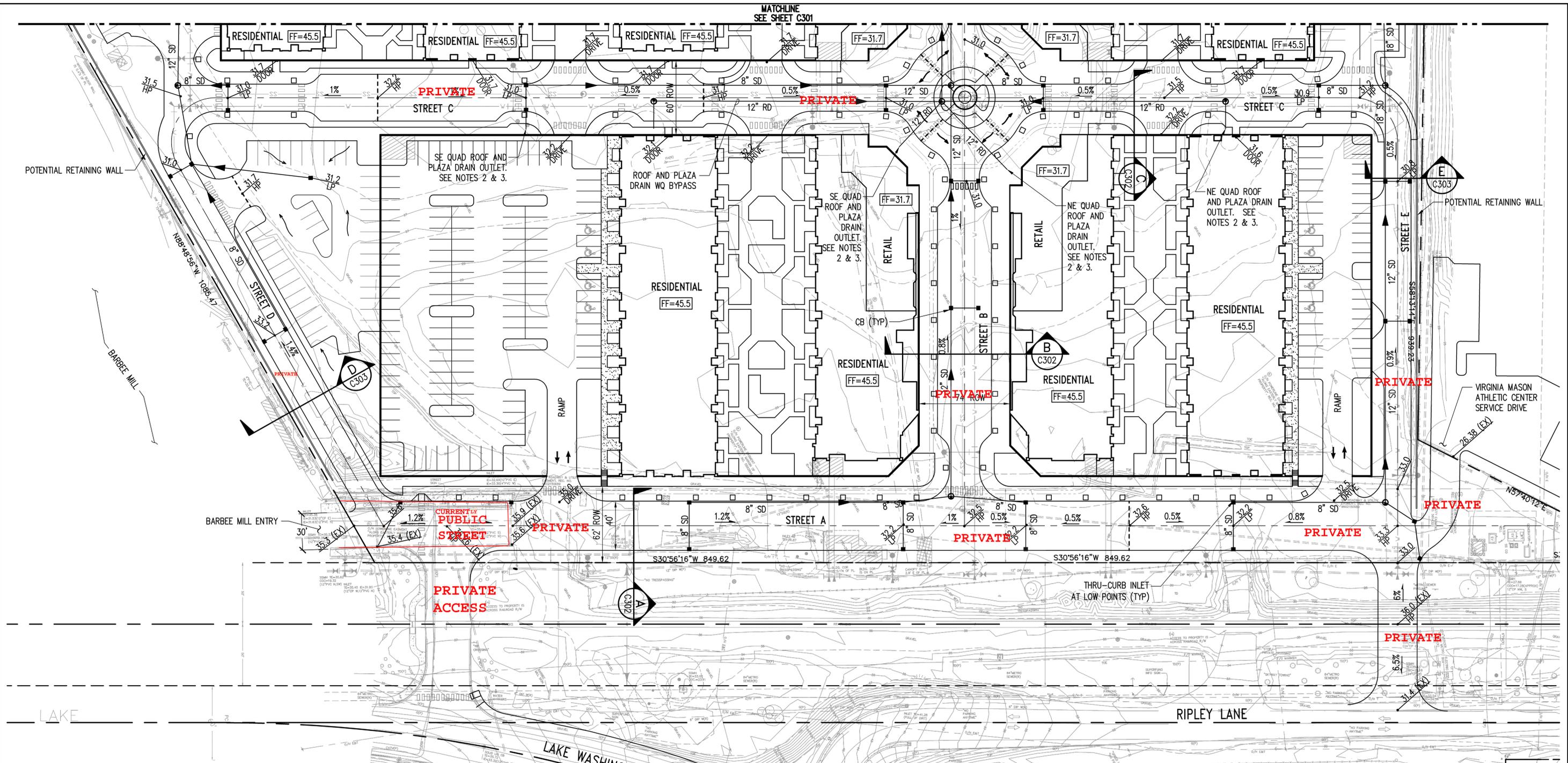
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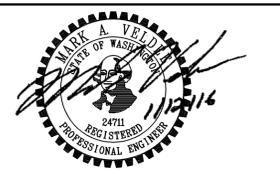
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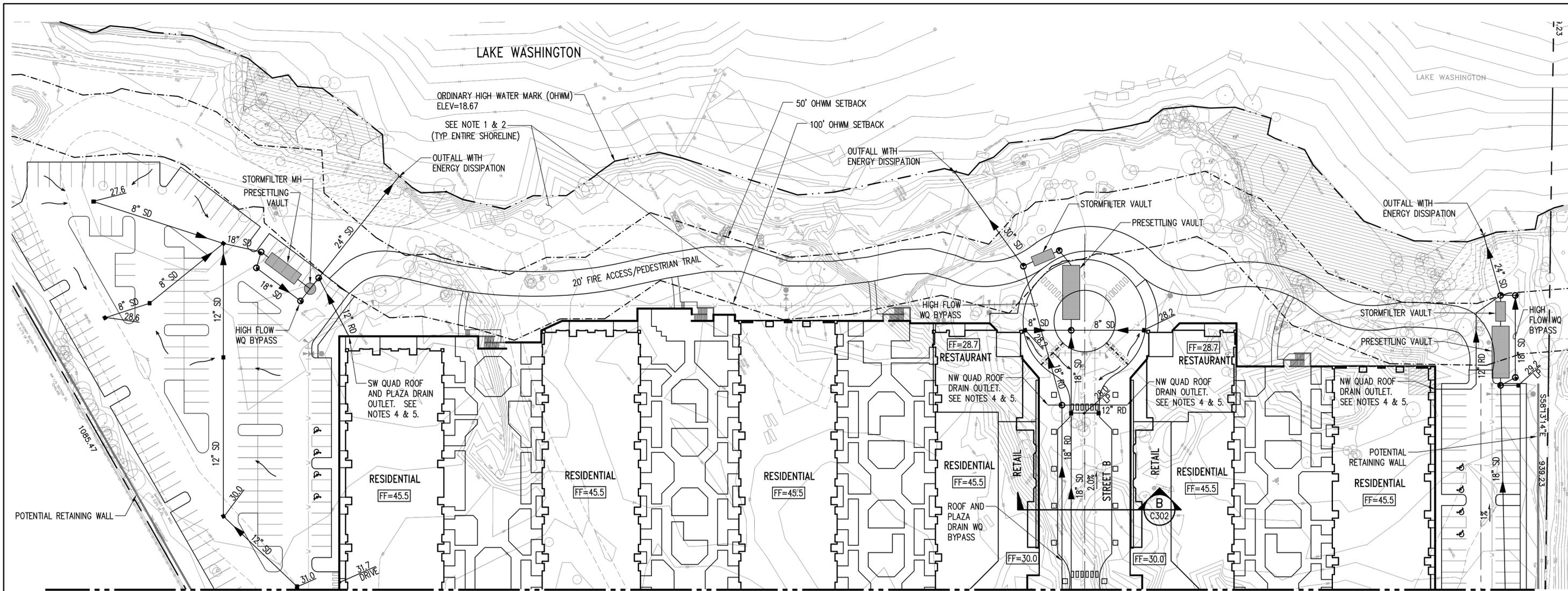
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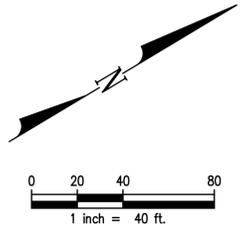
SEE SHEET C300
MACHLINE

LEGEND:

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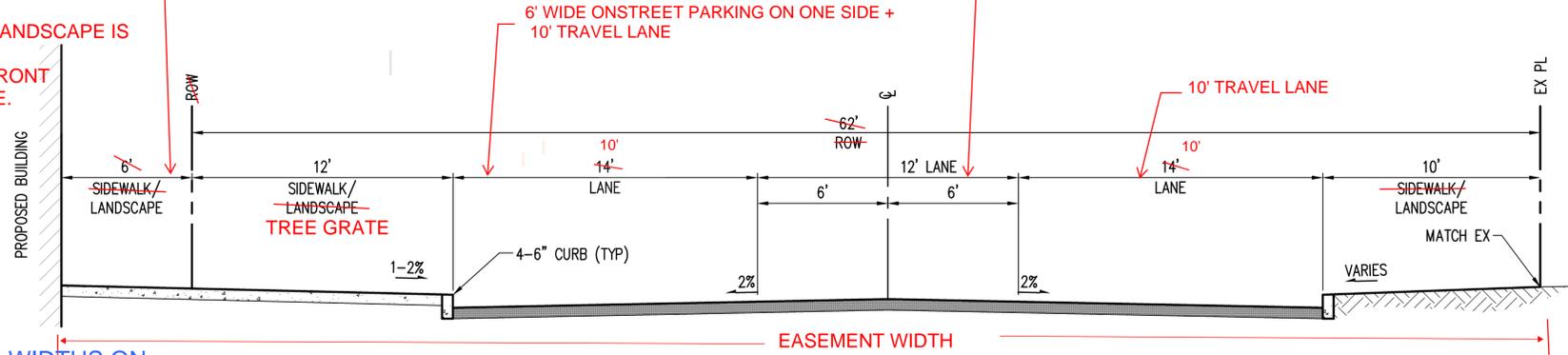
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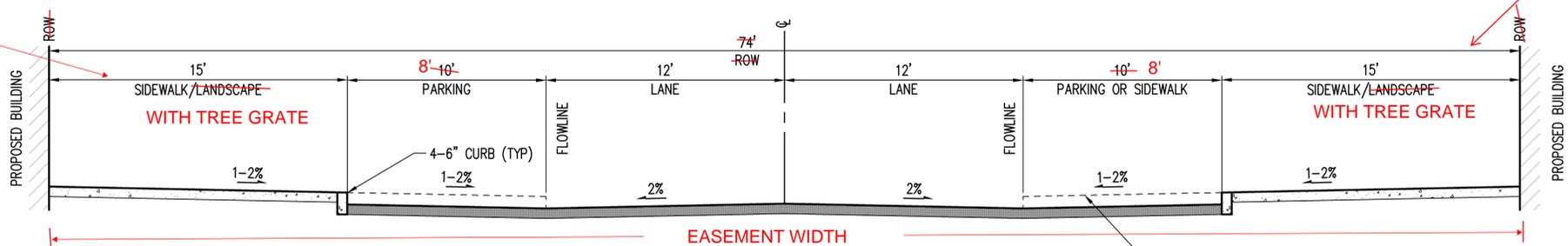


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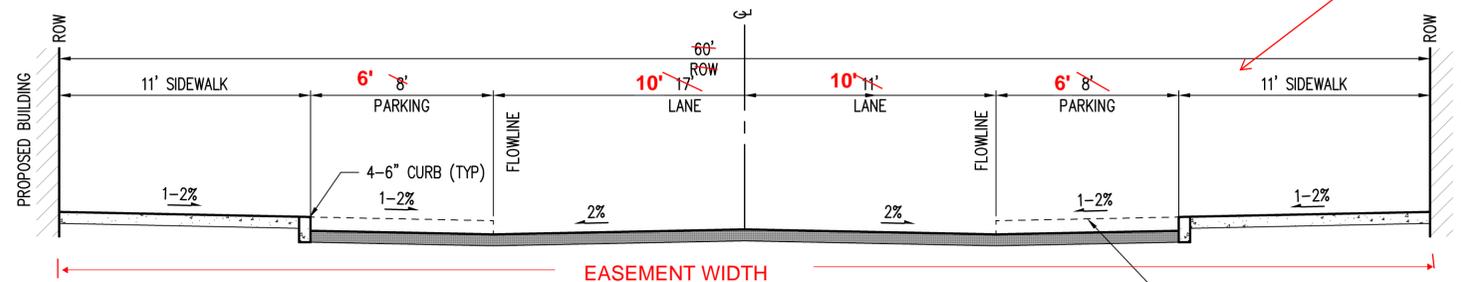
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STREET B - TYPICAL ROADWAY SECTION
SCALE: 1" = 4'

SIDEWALK EXTENDS TO ROADWAY EDGE WHERE NO ON-STREET PARKING, SEE PLAN (TYP)



THE CROSS SECTION ELEMENTS AND WIDTHS ON STREET C WILL VARY DEPENDING ON THE USE OF BUILDINGS ON EITHER SIDE OF THE STREET.

STREET C - TYPICAL ROADWAY SECTION
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SIDEWALK EXTENDS TO ROADWAY EDGE WHERE NO ON-STREET PARKING, SEE PLAN (TYP)

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ROADWAY SECTIONS

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M E M O R A N D U M

DATE: March 28, 2016

TO: Vanessa Dolbee, Current Planning Manager

FROM: Brianne Bannwarth, Development Engineering Manager

**SUBJECT: Traffic Concurrency Test – Quendall Terminals;
File No. LUA09-151**

The applicant is requesting Master Plan Review, Binding Site Plan, Shoreline Substantial Development Permit and SEPA Environmental Review for a mixed-use development located at 4350 Lake Washington Blvd. The site is 21.46 acres and is zoned Commercial/Office/Residential (COR) and located within the Urban Shoreline designation. The 21.46-acre site would be divided into 7 lots of which 4 would contain 6 - 7 story mixed-use buildings. Overall, the development would consist of 692 residential units (resulting in a net residential density of 40.95 units/acre), 20,025 square feet of retail and 9,000 square feet of restaurant. The applicant has proposed to dedicate 3.65 acres for public right-of-way, which would provide access to the 7 proposed lots. Surface and structured parking would be provided for 2,171 vehicles. The site contains approximately 0.81 acres of wetlands and 1,583 linear feet of shoreline along Lake Washington.

The proposed development would generate approximately 5,656 net new average weekday daily trips. During the weekday AM peak hour, the project would generate approximately 435 net new trips (104 inbound and 331 outbound). During the weekday PM peak hour, the project would generate approximately 530 net new trips (340 inbound and 190 outbound). The proposed project passes the City of Renton Traffic Concurrency Test per RMC 4-6-070.D as follows:

Traffic Concurrency Test Criteria	Pass
Implementation of citywide Transportation Plan	Yes
Within allowed growth levels	Yes
Project subject to transportation mitigation or impact fees	Yes
Site specific street improvements to be completed by project	Yes
Traffic Concurrency Test Passes	

Evaluation of Test Criteria

Implementation of citywide Transportation Plan: As shown on the attached citywide traffic concurrency summary, the city’s investment in completion of the forecast traffic improvements are at 130% of the scheduled expenditure through 2016.

Within allowed growth levels: As shown on the attached citywide traffic concurrency summary, the calculated citywide trip capacity for concurrency with the city adopted model for 2014 is 85,884 trips, which provides sufficient capacity to accommodate the 5,656 additional trips from this project. A resulting 80,228 trips are remaining.

Project subject to transportation mitigation or impact fees: The project will be subject to transportation impact fees at time of building permit for each new building.

Site specific street improvements to be completed by project: The project will be required to complete all internal and frontage street improvements for the building prior to occupancy. Any additional off-site improvements identified through SEPA or land use approval will also be completed prior to final occupancy.

Background Information on Traffic Concurrency Test for Renton

The City of Renton Traffic Concurrency requirements for proposed development projects are covered under Renton Municipal Code (RMC) 4-6-070. The specific concurrency test requirement is covered in RMC 4-6-070.D, which is listed for reference:

D. CONCURRENCY REVIEW PROCESS:

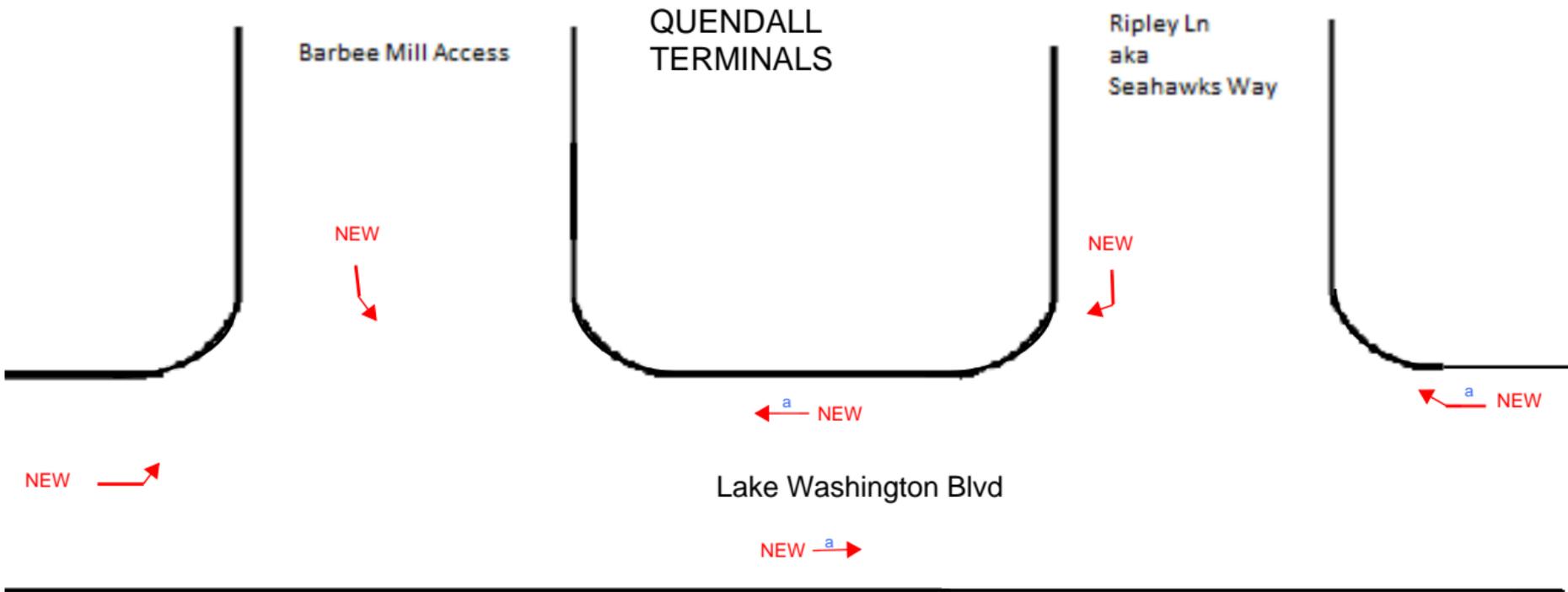
1. Test Required: A concurrency test shall be conducted by the Department for each nonexempt development activity. The concurrency test shall determine consistency with the adopted Citywide Level of Service Index and Concurrency Management System established in the Transportation Element of the Renton Comprehensive Plan, according to rules and procedures established by the Department. The Department shall issue an initial concurrency test result describing the outcome of the concurrency test.

2. Written Finding Required: *Prior to approval of any nonexempt development activity permit application, a written finding of concurrency shall be made by the City as part of the development permit approval. The finding of concurrency shall be made by the decision maker with the authority to approve the accompanying development permits required for a development activity. A written finding of concurrency shall apply only to the specific land uses, densities, intensities, and development project described in the application and development permit.*

3. Failure of Test: *If no reconsideration is requested, or if upon reconsideration a project fails the concurrency test, the project application shall be denied by the decision maker with the authority to approve the accompanying development activity permit application.*

The Concurrency Management System established in the Transportation Element on page XI-65 of the Comprehensive Plan states the following:

Based upon the test of the citywide Transportation Plan, consideration of growth levels included in the LOS-tested Transportation Plan, payment of a Transportation Mitigation Fee, and an application of site specific mitigation, development will have met City of Renton concurrency requirements.



a - Lane configuration on Lake Washington Blvd to be finalized after coordination with WSDOT.

ADDITIONAL MOTOR VEHICULAR LANES REQUIRED TO MITIGATE PROJECT IMPACTS
 (Included in DEIS, EIS Addendum, FEIS, or Mitigation Document)
EXHIBIT 18